

7.14 AMENDMENT C212 - PLANNING SCHEME UPDATES AND CORRECTIONS - DECISION GATEWAY 1

Director City Futures Kirsten Coster

City Strategy and Design

Officer Recommendation

That Council:

1. Using its powers as a planning authority under ss8A and 8B of the *Planning and Environment Act 1987*, seek authorisation from the Minister for Planning to prepare Moreland Planning Scheme Amendment C212 as detailed in Section 3 of this report and in Attachments 1, 2, 3, 4, 5 and 6 to this report.
2. Following receipt of the Minister's authorisation, exhibit the Amendment in accordance with Section 19 of the *Planning and Environment Act 1987* as outlined in the Consultation section of this report.
3. Authorises the Director City Futures to make changes to the Amendment based on conditions imposed in any authorisation granted by the Minister for Planning and to make minor changes.

REPORT

Executive Summary

Amendment C212 seeks to improve the operation of the Moreland Planning Scheme and planning permit decision making by introducing new policy content, new planning permit application requirements, and correcting a number of anomalies and errors/inconsistencies identified in the Moreland Planning Scheme.

In 2018, in accordance with section 12B of the *Planning and Environment Act 1987*, Council officers undertook a statutory review of the Moreland Planning Scheme. The *Planning Scheme Review Report 2018* (DED38/18) was subsequently adopted by Council on 13 June 2018. Amendment C212 to the Moreland Planning Scheme proposes to give effect to some review recommendations, including:

- recognising the Brunswick Design District in the Municipal Strategic Statement to become a successful and innovative employment and creative precinct. *Amendment C212 makes changes to the Municipal Planning Strategy in support of this recommendation.*
- To ensure the Planning Scheme supports the Virtual Moreland Program by including requirements to provide 3D modelling for major developments. *Amendment C212 introduces new requirements for planning permit applications in the Brunswick and Neighbourhood Activity Centres to provide 3D models.*

Amendment C212 also proposes to make changes to the Municipal Planning Strategy and Planning Policy Framework to implement new policy directions from Council strategies adopted post the last review.

The Amendment also seeks to correct anomalies that have been identified throughout the Moreland Planning Scheme by fixing ordinance, mapping and clerical errors. It also deletes two Design and Development Overlays that are no longer required as construction is complete and no further design guidance is required.

Previous Council Decisions

DCF33/20, Moreland Planning Policy Framework Translation (Amendment C200) and Planning Scheme Anomalies (Amendment C199) – Decision Gateway 1 – 12 August 2020

That Council:

1. *Endorses the draft Municipal Planning Strategy and local policies at Attachment 2 to this report.*
2. *Requests the Minister for Planning to prepare, adopt and approve Moreland Planning Scheme Amendment C200 pursuant to section 20(4) of the Planning and Environment Act 1987 as shown in Attachment 2 to this report.*
3. *Requests the Minister for Planning to prepare, adopt and approve prescribed Moreland Planning Scheme Amendment C199 pursuant to section 20A of the Planning and Environment Act 1987 at Attachment 3.*

DED38/18, Statutory Requirement – Planning Scheme Review – 13 June 2018

That Council:

1. *Endorses the Moreland Planning Scheme Review 2018 Report, at Attachment 1 to this report.*
2. *Submits the Moreland Planning Scheme Review 2018 report to the Minister for Planning in accordance with section 12B (1) of the Planning and Environment Act 1987.*

1. Policy Context

Council Plan

The Council Plan 2017-2021 (Council Plan) seeks to create a progressive city. The Moreland Planning Scheme (Planning Scheme) underpins delivery of the Council Plan through directing land use and development in Moreland. Implementing recommendations from the *Planning Scheme Review Report 2018* (the PSR) and introducing new content into the Municipal Planning Strategy (MPS) and Planning Policy Framework (PPF) will help to achieve the following priorities of the Council Plan:

- P1 – Enhance liveability, affordability and sustainability by guiding growth, and excellence in urban design and development, specifically deliverables P1e - Implementation of housing policy implementation and P1a - Virtual Moreland.
- P4 – Increase tree canopy cover, enhance existing open space and create at least two new parks in areas with the lowest access to open space. The amendment will introduce new policy into the MPS/PPF aimed at implementing the *Urban Forest Strategy* and *Nature Plan*, as well as clearly identifying high, medium and low priority areas for open space in accordance with a *Park Close to Home*.
- P3 – Support the local economy and trading environments to enhance economic activity and promote local jobs. The Amendment will implement the Brunswick Design District into the MPS and PPF to support this precinct becoming a successful and innovative employment precinct as well as providing support for creative industries in Core Industrial areas and activity centres.

Removing errors and inconsistencies identified throughout the Planning Scheme will also help to achieve a progressive city by improving planning outcomes and removing unclear planning requirements.

2. Background

Planning Scheme Review Report 2018

Section 12B of the *Planning and Environment Act 1987* (the Act) requires a planning authority to review its planning scheme no later than one year after the approval of the Council Plan. The PSR was adopted by Council on 13 June 2018 and found that the Planning Scheme was operating well as significant improvements had been made in recent years prior to the review. However, to ensure that the Planning Scheme continued to support the delivery of a sustainable, liveable Moreland and reflected recently adopted policies, strategies and State Government reforms, the PSR made a number of recommendations to improve and update the Moreland Planning Scheme.

Amendment C200

Amendment C200 was adopted by Council on 12 August 2020 and came into effect in the Scheme on 14 January 2021. It replaced the existing Local Planning Policy Framework (LPPF) with the MPS and PPF in line with the State Government's Smart Planning Reforms aimed at making planning schemes more efficient, accessible and transparent. Amendment C200 was not subject to public notification as the translation was policy neutral.

Amendment C200 partially implemented the PSR by improving the structure and operation of the Planning Scheme, it did not introduce any new content.

3. Issues

Amendment C212 proposes to implement recommendations from the PSR, by introducing new policy content endorsed by Council, removing overlays that are no longer required and correcting a number of anomalies and errors/inconsistencies identified throughout the Planning Scheme. Further details of the changes proposed are listed below.

3.1 Implementation of Planning Scheme Review Report 2018 Recommendations

3.1.1 Recommendation 2P

Recommendation 2P of the PSR recommends undertaking a review of all zones, Design and Development Overlays (DDOs) and other overlays that guide buildings and works with a focus on the findings at Appendix 3 of the Report.

The Amendment proposes to delete the following overlays that were developed to require specific pedestrian and vehicle access arrangements (now delivered) and as shown in the maps at Attachment 1:

Overlay	Location/Application	Reason for Deletion
DDO7 The DDO sets out to ensure the site is integrated with the surrounding neighbourhood by providing for public, bicycle and pedestrian access between John and Miller Streets. It also contains objectives to create an attractive and communal public realm through clear delineation between public and private areas, landscaping and other streetscape design elements, pedestrian priority and safe pedestrian	2 John Street and 12-20 Miller Street, Brunswick East	The intent of the DDO was to ensure any development provided for a public road linking Miller and John Streets, car parking to the rear of dwellings and integrated landscaping which provided public access in accordance with the <i>Miller Street and John Street Connection Plan</i> attached to the DDO. This has been fulfilled with a development and road constructed in accordance with the Connection Plan. There is no longer a need to include the DDO on the land.

Overlay	Location/Application	Reason for Deletion
environments through laneway access to vehicle parking.		
DDO9 The DDO seeks to ensure residential and commercial development on the site is integrated with the surrounding neighbourhood by providing for public, bicycle and pedestrian access, including public pedestrian access from Walter Street to Middle Street.	2-6 & 8-10 Walter Street, Hadfield	The intent of the DDO was to ensure public pedestrian access from Walter Street to Middle Street which has been fulfilled by the construction of a road. Permits have also been issued for subdivision and development of the sites and development is nearly completed. There is no longer a need to include the DDO on the land.

3.1.2 Recommendations 2IND and 4IND of the *Planning Scheme Review Report 2018*

Recommendation 2IND of the PSR seeks to recognise the Brunswick Design District (BDD) in the Municipal Strategy Plan (formerly Municipal Strategic Statement, MSS) and support the precinct to become a successful and innovative employment and creative precinct.

The BDD, a collaborative partnership between Council, RMIT University and Creative Victoria, is an emerging employment and creative precinct located in Brunswick for creative industries and includes the Brunswick Campus of RMIT, the Dawson Street police garage site (a former hat factory), Jewell Station reserve, the Brunswick Design and Innovation centre (BRUDI), the Brunswick Civic and Cultural Precinct, Counihan Gallery, The Brosnan Centre (Jesuit Social Services) and 33 Saxon Street (Siteworks).

Similarly, Recommendation 4IND seeks for the planning scheme to support creative industries that wish to expand or establish in Core Industrial Areas. Creative industries include sectors spanning arts, culture, screen design, publishing and advertising such as museums, libraries, performing arts, music and fashion. The *Moreland Arts Infrastructure Plan 2018* identifies the importance of the Core Industrial Areas to creative industries due to industrial areas and buildings having the ability to be easily retrofitted as creative spaces and the level of protection industrial zoning provides for art spaces.

The *Moreland Arts Infrastructure Plan 2018* also highlights that, alongside industrial areas, the majority of creative industries are also operating within commercial areas, primarily in the Brunswick. In providing support for emerging creative industries in the Planning Scheme, it is also considered that this should extend to activity centres to help provide some level of protection for creative industries that wish to establish or expand in these areas.

To implement recommendations 2IND and 4IND of the PSR, the Amendment proposes to make the following changes to the planning scheme as shown at Attachment 2:

- Amend Clause 2.03-6 Economic Development of the MPS to insert the following text in the context section: 'To support the establishment of these growing industries, Council has partnered with RMIT University and Creative Victoria to establish the Brunswick Design District, a distinct design and creative hub for creative and design professions located primarily in the heart of Brunswick. While key sites within the District have helped define the District's core area, the activity of the District will have influence beyond Brunswick and help support a thriving and resilient creative community and economy throughout the municipality and Melbourne's North.'

- Amend Clause 2.03-6 Economic Development of the MPS to insert the following strategic direction: 'Supporting creative industries, entrepreneurs and associated activities, including education and training, live music venues, performance, studios and coworking spaces in Activity Centres, the Brunswick Design District and Core Industrial and Employment Areas as shown on the Strategic Framework Plans *Economic Development* and *Activity Centres* at Clause 2.04'.
- Amend the Strategic Framework Plan: *Economic Development* at Clause 2.04 of the MPS to show the Brunswick Design District.

3.1.3 Recommendation 4UD

The Virtual Moreland program uses 3-Dimensional (3D) mapping, 3D modelling, virtual reality and augmented reality to help improve understanding of how a proposed development will impact on the streetscape. This enables more robust and informed decisions and improved community consultation on development in Moreland.

Recommendation 4UD of the PSR seeks to ensure that the Scheme supports the Virtual Moreland program by including requirements for major developments to provide 3D models as part of the application process (at lodgement and approval stages).

The Planning Scheme currently includes application requirements within the Coburg Activity Centre Zone for developments of four or more storeys to provide 3D models of the development. However, there is no such requirement for developments within the Brunswick Activity Centre or Neighbourhood Centres. As it is expected that the majority of developments where a 3D model will assist decision making and consultation will occur within major and neighbourhood activity centres, the Amendment proposes to make the following changes to the Planning Scheme as shown in Attachment 3 to implement Recommendation 4UD:

- Introduce application requirements requiring the submission of 3D models for developments of four or more storeys in the following Design and Development Overlays (DDOS):
 - DDO18 (Brunswick Activity Centre – Sydney Road and Upfield Corridor);
 - DDO19 (Brunswick Activity Centre – Lygon Street Local Area);
 - DDO20 (Brunswick Activity Centre – Nicholson Street Local Area);
 - DDO22 (Land Bound by Barkly Street to North, Nicholson Street to the East, Brunswick Road to the South, Brunswick East);
 - DDO23 (Balfe Park Precinct, Brunswick East); and
 - DDO24 (Neighbourhood Centres).

3.2 New Policy Content

As part of the translation of the existing LPPF into the MPS and PPF, new policy content for the Planning Scheme was identified. This content is drawn from strategies adopted between the 2015 PSR and the 2018 PSR. Specifically, policy direction from the following strategies is proposed to be included in the planning scheme: *Urban Forest Strategy 2017-2022*, *A Park Close to Home 2017*, the *Affordable Housing Action Plan 2019/20* and the *Moreland Integrated Transport Strategy 2019* (including recent changes to this strategy noted in Council report 7.1 from the 14 April 2021 Council meeting) summarised in the table below and shown in full at Attachment 4:

Clause No.	Proposed Change
Municipal Planning Strategy	
2.03-2 Environment and Landscape	<ul style="list-style-type: none"> ▪ Deleted the introduction as the wording does not reflect that Moreland does not have significant vegetation and trees. ▪ Additional wording included in the context in support of actions to

Clause No.	Proposed Change
Values	<p>address the urban heat island effect.</p> <ul style="list-style-type: none"> Change to strategic direction to include additional wording to give effect to Council's <i>Urban Forest Strategy</i> and <i>Nature Plan</i>.
2.03-7 Transport	<p>Makes the following changes to support the implementation of the <i>Moreland Integrated Transport Strategy 2019</i>:</p> <ul style="list-style-type: none"> Delete the following wording from the context section: <i>'To retain Moreland's liveability and manage transport issues associated with a denser built form, car parking will need to respond and adapt to a growing and changing Moreland'</i>. Insert the following wording in the context section: <i>'The availability of car parking where people live, and their destinations, will strongly influence the ways they travel. Getting the type, location and amount of car parking right can contribute to better transport, land use, economic and community outcomes. This includes improved sustainable transport uptake while catering for those who need to drive.'</i> Delete the existing strategic directions and replace with the following: <ul style="list-style-type: none"> Planning for a transport network that: <ul style="list-style-type: none"> Caters for all ages, is accessible and equitable, including recognition that some people need to drive. Reduces local vehicle traffic and safeguards the wellbeing of the community. Achieves a shift towards sustainable modes of travel, including a transition to active transport or zero-emissions transport. Focuses on transport safety, improving personal security and safety. Connects people to local jobs and services. Caters for population and employment growth. Prioritising our transport network according to the following 'road user hierarchy', while ensuring access for those who need to drive: <ul style="list-style-type: none"> Pedestrians. Cyclists. Public transport. Private vehicles.
2.04 Open Space Strategic Framework Plan	<ul style="list-style-type: none"> Updated to show High, Medium and Low Priority Open Space Gap Areas in line with Council's <i>A Park Close to Home</i> strategy. Updated to show following sites purchased as part of <i>Park Close to Home</i> as public open space: <ul style="list-style-type: none"> 14 Frith Street, Brunswick. 132-134 Cardinal Road, Glenroy. 41-43 Service Street, Coburg. 260 Sydney Road, Brunswick.
Planning Policy Framework	
12.01-1L Biodiversity	<p>Insert the following strategies to support the implementation of the <i>Urban Forest Strategy</i> and <i>Nature Plan</i>:</p>

Clause No.	Proposed Change
	<ul style="list-style-type: none"> Encourage the planting and retention of trees in the private realm. Increase the planting and retention of native vegetation to enhance and create habitat corridors. Encourage a diversity of vegetation to promote climate pest and disease resilience.
15.01-1L Urban Design	<p>Insert the following strategy to support the implementation of the <i>Moreland Integrated Transport Strategy 2019</i>:</p> <ul style="list-style-type: none"> Reallocate road space and existing car parking to create greener streets, such as through street tree planting.
15.01-3L Heritage	<p>Heritage guidelines have been updated to reference the new PPF clauses in the planning scheme and with Council's new branding. Insert a new policy guideline:</p> <ul style="list-style-type: none"> The design approaches for fences contained in the <i>Guidelines for Fences in Heritage Areas</i>, Moreland City Council 2021. <p>Insert the following two additional documents under 'Policy Documents':</p> <ul style="list-style-type: none"> <i>Guidelines for Fences in Heritage Areas</i>, Moreland City Council, 2021. <i>Guidelines for Assessing Planning Permit Applications</i>, Heritage Victoria, 2007. <p>Updates the date of the following existing document under 'Policy Documents' from 2001 to 2021 to ensure the current version is referenced:</p> <ul style="list-style-type: none"> <i>Guidelines for alterations and additions to detached houses in heritage areas</i>, Moreland City Council, 2021.
16.01-4L Housing Affordability	<p>Insert the following strategy to support of the implementation of Council's <i>Affordable Housing Action Plan</i>:</p> <ul style="list-style-type: none"> Encourage public landowners to use surplus land for the provision of social and affordable housing.
17.01-1L Core Industry and Employment Areas	<p>Amend the following strategy to provide clarification of the location of 420 Victoria Street, Brunswick:</p> <p>'Support 420 Victoria Street, Brunswick (formally known as the Brunswick Business Incubator at 406 Victoria Street, Brunswick) within the <i>Brunswick Core Industry and Employment Area</i> as a facility for entrepreneurs and enterprises'</p>
Clause 18.02-1L Sustainable Transport in Moreland	<p>Insert the following strategy to support the implementation of the <i>Moreland Integrated Transport Strategy 2019</i>:</p> <ul style="list-style-type: none"> Reallocating road space and existing car parking to: <ul style="list-style-type: none"> Support improvements to pedestrian and cycling infrastructure, including access to public transport. Facilitate the safety of walking and cycling.
Clause 18.02-2L Public Transport in Moreland	<p>Insert a new local policy with the following strategy to support the implementation of the <i>Moreland Integrated Transport Strategy 2019</i>:</p> <ul style="list-style-type: none"> Reallocating road space and existing car parking to: <ul style="list-style-type: none"> Support improvements to public transport.

Clause No.	Proposed Change
Operational Provisions	
Schedule to 72.08 Background Documents	<ul style="list-style-type: none"> Content updated to reference up-to-date strategies. Outdated strategies deleted.

Content has also been re-introduced into the PPF where it was deleted in error during the translation of Council's LPPF into the MPS and PPF. The content is summarised in the table below and shown in full at Attachment 4:

Clause No.	Proposed Change
Planning Policy Framework	
11.03-1L Activity Centres	<p>Create a new local policy with the following strategy and policy guideline. The strategies are from Strategies 3.2 and 3.3 of Clause 21.03-3 of the existing Municipal Strategic Statement (MSS) and were inadvertently deleted in error during the translation.</p> <p><u>Strategies:</u></p> <ul style="list-style-type: none"> Ensure residential uses do not undermine the viability of businesses operating in activity centres. Support the continued operation of existing noise generating uses. <p><u>Policy guidelines:</u></p> <p>Consider as relevant:</p> <ul style="list-style-type: none"> An activity centre's role to accommodate a mix of uses with day and night time activity. Consideration of residential amenity impacts for dwellings located within and directly adjacent to an activity centres must be tempered to ensure the role of the activity centre is not undermined.

3.3 Rezoning of Public and Privately-Owned Land

The Amendment proposes to correct zoning anomalies for public and privately-owned land. This includes land purchased by Council for new parks as part of *A Park Close to Home*. A list of the properties to be rezoned is provided below and included at Attachment 5.

Affected Land	Proposed Change	Reason for Change
6 McDonald Street, Coburg North	Rezone land to Industrial 1 Zone (IN1Z) as shown on part of Planning Scheme Maps 8 and 11	The land is currently zoned Public Use Zone Schedule 4 – transport (PUZ4) with a small portion zoned Industrial 3 (IN3Z). The PUZ is a public land zone and Schedule 4 relates to public land set aside for transport purposes. A planning scheme can only include land in a public land zone if the land is Crown land or is owned, vested in or controlled by a Minister, government department, public authority or a municipal Council. The land was previously owned

Affected Land	Proposed Change	Reason for Change
		<p>by the Railways Department however was sold in 1995 to private landowners and has remained privately owned since. The land currently appears to be used as a factory for steel products construction.</p> <p>The land is located adjacent to a Core Industrial Area identified in Moreland's Industrial Land Strategy (MILS). The IN1Z is considered appropriate given the sites location adjacent to the Core Industrial Area; it will be consistent with zoning of land directly opposite on the other side of McDonalds Street; and the current use of the site for steel product construction.</p>
41-43 Service Road, Coburg	Rezone land to Public Park and Recreation Zone (PPRZ) as shown on part of Planning Scheme Maps 8 and 11	The land is currently zoned General Residential Zone (GRZ) however was purchased for new parkland by Moreland City Council as part of <i>Park Close to Home</i> implementation.
1, 3 & 7 Leonard Street, Fawkner	Rezone land to PPRZ as shown on part of Planning Scheme Map 5	The land is currently zoned GRZ however forms part of existing parkland.
43 Kernan Street, Pascoe Vale South	Rezone land to PPRZ as shown on part of Planning Scheme Maps 7 and 10	The land is currently zoned Neighbourhood Residential Zone (NRZ) however forms part of existing parkland.
33 Outlook Drive, Glenroy	Rezone part of the land to PPRZ as shown on Part of Planning Scheme Maps 1 and 2	The land is partly zoned NRZ however forms part of existing parkland.
2 Derby Street, Fawkner	Rezone land to PPRZ as shown on Planning Scheme Map 9	The land is currently zoned NRZ however forms part of existing parkland.
104-110 Newlands Road, Coburg North	Rezone land to PPRZ as shown on part of Planning Scheme Map 9	A portion of the land is currently zoned IN1Z however forms part of existing parkland.
1 West Street, Brunswick	Rezone land to PPRZ as shown on part of Planning Scheme Map 14	<p>The land is currently zoned Commercial 1 Zone (C1Z) however was purchased for new parkland as part of <i>Park Close to Home</i> implementation.</p> <p>It is also proposed to extend the Environmental Audit Overlay (EAO) currently affecting parts of the land (the land was consolidated from a number of different parcels) over the whole</p>

Affected Land	Proposed Change	Reason for Change
		site to avoid confusion over where the EAO applies.
55-61 Tinning Street, Brunswick	Rezone land to PPRZ as shown on part of Planning Scheme Map 14	The land is currently zoned IN3Z however was purchased for new parkland as part of <i>Park Close to Home</i> implementation. Given that the land was previously zoned IN3Z, an EAO will also be applied to the land to account for any sensitive uses that may be proposed on the site in the future.
14 Frith Street, Brunswick	Rezone land to PPRZ as shown on part of Planning Scheme Map 14	The land is currently zoned Mixed-Use Zone (MUZ) however was purchased for new parkland as part of <i>Park Close to Home</i> implementation.
260 Sydney Road, Brunswick	Rezone land to PPRZ as shown on part of Planning Scheme Map 14	The land is currently zoned C1Z however was purchased for new parkland as part of <i>Park Close to Home</i> implementation. Given that the land was previously zoned C1Z, an EAO will also be applied to the land to account for any sensitive uses that may be proposed on the site in the future.
132-134 Cardinal Road, Glenroy	Rezone land to PPRZ as shown on Part of Planning Scheme Map 3	The land is currently zoned NRZ however was purchased for new parkland as part of <i>Park Close to Home</i> implementation.
Lygon Street, between Brunswick Road and Park Street, Brunswick	Rezone land to part C1Z and PPRZ as shown on part of Planning Scheme Map 15	The road is currently zoned Road Zone 1 which is inconsistent with the rest of Lygon Street which is zoned according to the zoning of adjoining land. The road is also not a declared road by VicRoads.

3.4 Other Anomalies/Errors to be Corrected

A number of other anomalies/errors have been identified throughout the Planning Scheme and require correction. These have been identified through the day to day use of the Moreland Planning Scheme and if left unaddressed will lead to confusion for both the community and Council officers administering the Planning Scheme. A list of these anomalies is provided below and included at Attachment 6.

Description of Error	Affected Document	Change Proposed
Clause 17.01-1L Employment Areas incorrectly names the Strategic Framework Plan referred to under 'Policy Application' as 'Employment' and not 'Economic	17.01-1L Employment Areas	Update the reference to: 'Strategic Framework Plan: <i>Economic Development</i> '.

Development'		
Design and Development Overlay – Schedule 16 references the outdated <i>Moreland Industrial Land Use Strategy</i> at section 7.0 Reference Documents	DDO16	Update reference to: <i>Moreland Industrial Land Strategy 2015-2030</i> , Moreland City Council, July 2016
Heritage Overlay 246 currently applies to half of 20 Dawson Street, Brunswick. The <i>Moreland Heritage Gap Study 2019</i> recommended that the heritage overlay should cover the entirety of the site as it currently cuts through the building.	Planning Scheme Map 14HO	<ul style="list-style-type: none"> Amend Planning Scheme Map 14HO to apply HO246 to the entirety of 20 Dawson Street, Brunswick. Amend the Schedule to 43.01 (Heritage Overlay) and Schedule to 72.04 (Incorporated Documents) to introduce a new statement of significance for 20 Dawson Street, Brunswick.

Human Rights Consideration

The implications of this report have been assessed in accordance with the requirements of the Charter of Human Rights and Responsibilities and there are no adverse impacts. The human rights considered as part of the preparation of this report relate specifically to freedom, dignity and equity.

The most relevant section of the Victorian Charter of Human Rights to the preparation of this report is property rights. This report does not deprive any party of any legal or proprietary interest in land, or the ability to use an develop that land in accordance with the planning regulatory framework. The amendment, if authorised, will go through a statutory process that includes public exhibition and consideration of submissions.

Gender Equality Considerations

A gender impact assessment has not been undertaken for the Amendment because it does not relate to a new policy, program or service that directly or significantly impacts on the public. The changes proposed through the Amendment correct identified errors and anomalies or introduce policy directions that implement existing strategies adopted by Council and which seek to improve planning outcomes throughout Moreland, for the benefit of everyone. If these strategies are to be reviewed in the future a gender impact assessment would be required as part of the strategy review process. The Amendment will also consider the feedback of all genders and how their land use needs are related to other areas of potential discrimination or marginalisation.

4. Community consultation and engagement

Public consultation will be conducted as part of the formal statutory exhibition process for the Amendment. Subject to the timing of the Minister's authorisation of the Amendment it is anticipated that exhibition will occur in August/September 2021 for a period of four weeks after notice of the Amendment is published in the Government Gazette and in the newspaper circulating the area.

In accordance with section 19 of the *Planning and Environment Act 1987* (the Act), notice of the Amendment via direct mail will be given to:

- Owners and occupiers of affected properties where changes are proposed under sections 3.1.1, 3.3 and 3.4 (20 Dawson Street, Brunswick only).
- Relevant state government departments and Ministers;

- All abutting Municipal Councils;
- Regular applicants (developers, architects and planning consultants) within Moreland who need to be aware of new application requirements in the DDOs specified under section 3.1.3.

To comply with other notice requirements in section 19 of the *Planning and Environment Act 1987* and to reach the wider community, the following will be provided throughout the exhibition process:

- Notice of the preparation of the Amendment in the Government Gazette and newspapers The Age and Herald Sun;
- Communication through Council's websites and social media platforms;
- Copies of exhibition material, including background material, will be available on Council's website and hard copies will be posted to residents upon request;
- Copies of Amendment documentation, including all background material, will be available on Council's website and at Moreland Service Centres and Libraries for the public to access (if these facilities are open at the time of exhibition).
- Notice in any relevant Council publication printed and available at the time of exhibition.

Under section 19(1A) of the Act, it is considered impractical to individually notify owners and occupiers throughout the municipality of changes proposed under sections 3.1.2 and 3.2 of this report as well as the owners and occupiers of the DDOs proposed to be changed under section 3.1.3. Furthermore, the changes proposed under section 3.1.3 are a technical requirement relating to the assessment of planning permit applications and do not change the land use and development outcomes currently allowed under the overlays.

Consultation has occurred with State Government departments that own land proposed to be rezoned to PPRZ through the Amendment.

5. Officer Declaration of Conflict of Interest

Council officers involved in the preparation of this report have no conflict of interest in this matter.

6. Financial and Resources Implications

Financial

Administrative costs associated with the processing of Amendment C212 will be met within the base budget of the Strategic Planning Unit.

Resources

It is anticipated that the amendment will come into effect in the 2022/2023 financial year. The impacts on the Urban Planning Unit at this time will be minimal as the Amendment does not introducing any new planning permit requirements that would trigger the need for a planning permit that is not already required under the Scheme. Furthermore, the fixing of anomalies and errors will remove time spent responding to unclear planning enquiries.

7. Implementation

If the officer recommendation is endorsed, the next step will be to seek authorisation to prepare the Amendment (as drafted) from the Minister for Planning. Following receipt of authorisation, the Amendment will be placed on statutory exhibition.

The following timeline is broken down into key 'decision' gateways for Council. The timeline is approximate and subject to Ministerial approval timelines and Planning Panels Victoria reporting and assumes an August/September 2021 exhibition period.

Decision Gateway 1: Authorisation and exhibition (current report)
Decision Gateway 2: Submission review and panel request (October/November 2021)
Decision Gateway 3: Panel report review and approval request (April/May 2022)

Attachment/s

1	Overlays to be Deleted	D21/103671
2	Brunswick Design District Changes to MPS	D21/103657
3	Virtual Moreland Changes	D21/103679
4	MPS and PPF Changes	D21/103685
5	Zoning Maps	D21/103691
6	Other Anomalies	D21/103694

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

- D-DDO - Area to be deleted from a Design and Development Overlay
- Local Government Area

Disclaimer

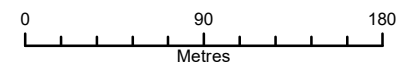
This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group

Print Date: 6/04/2021

Amendment Version: 3



Environment,
Land, Water
and Planning

Part of Planning Scheme Map 15DDO





MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  D-DDO - Area to be deleted from a Design and Development Overlay
-  Local Government Area



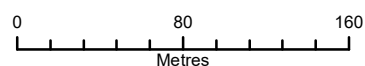
Part of Planning Scheme Maps 3DDO & 4DDO

Disclaimer

This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2020

Planning Group
Print Date: 13/10/2020
Amendment Version: 1



Environment,
Land, Water
and Planning

02.03-6 Economic development

C212

Moreland's economy is in transition, moving from manufacturing to a more service-based economy. This is evident in the types of industries and jobs available in the municipality, with health care and social assistance the largest employers followed by retail. However, the continued presence of industry and other commercial businesses within the municipality is important in creating a sustainable economy and a diversity of employment opportunities.

Moreland's industrial land plays a very important role in the provision of jobs and wealth generation and it is predicted that there will be a continued demand for industrial zoned land in Moreland from a range of industry sectors, despite the demand for industrial land declining over the last 30 years.

Moreland is also at the forefront of new ways of working and creative industries such as arts and performance production, tourism, clothing, jewellery and furniture design, digital arts and application development and creative professional services. These growing industries will play a key part in creating sustainable economic development throughout the municipality.

To support the establishment of these growing industries, Council has partnered with RMIT University and Creative Victoria to establish the Brunswick Design District, a distinct design and creative hub for creative and design professions located primarily in the heart of Brunswick. While key sites within the District have helped define the District's core area, the activity of the District will have influence beyond Brunswick and help support a thriving and resilient creative community and economy throughout the municipality and Melbourne's North.

There is increasing pressure, especially in industrial areas located adjacent to public transport and activity centres, for the conversion of industrial buildings into residential use. Residential encroachment into industrial areas displaces industrial uses and can create problematic interface issues.

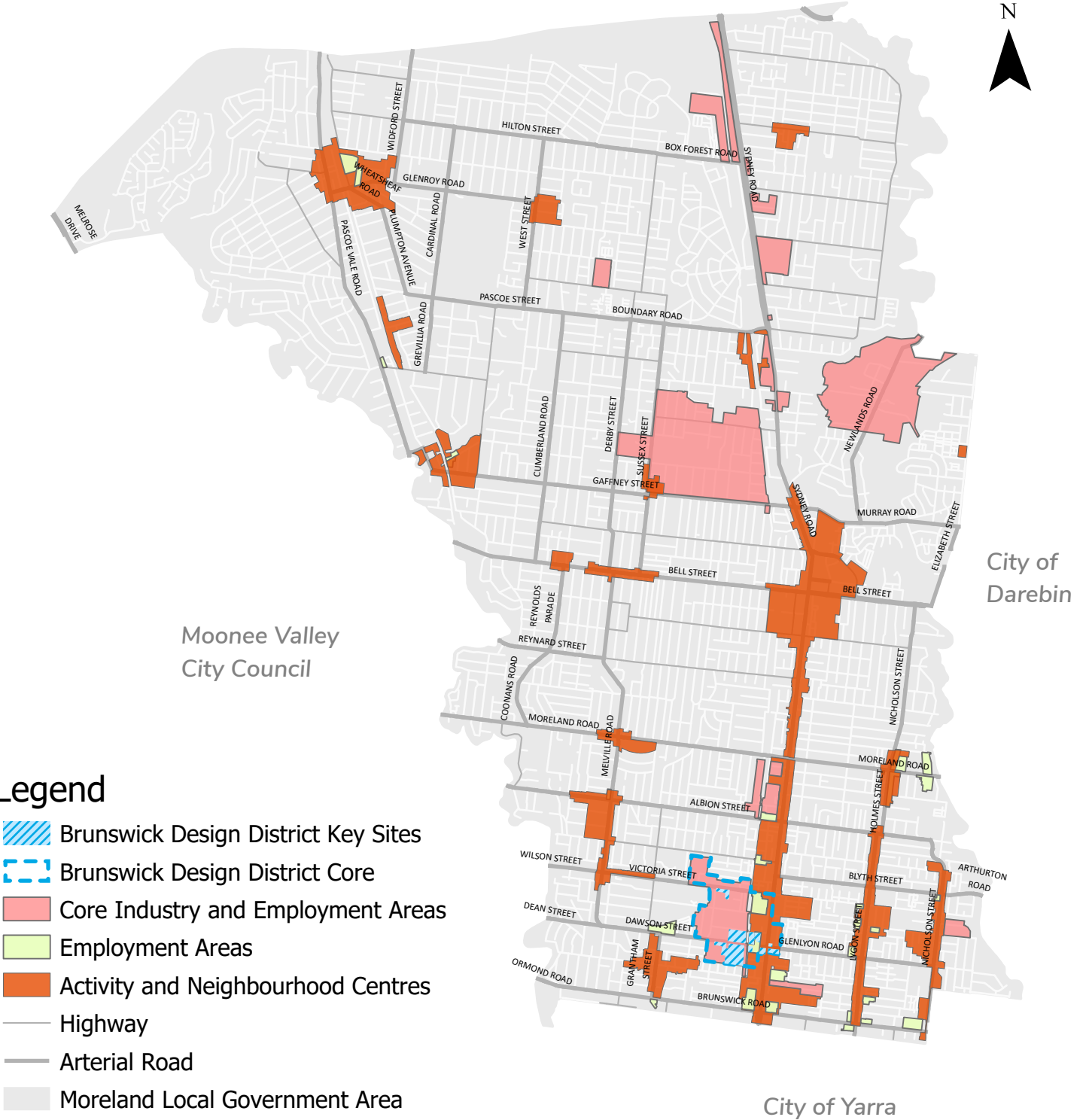
The vast majority of Moreland residents travel outside the municipality to work in nearby areas. There is a desire to provide greater opportunities for residents to work locally to reduce travel times to work, reduce congestion and for the sustainability and health benefits of less car reliance.

Council seeks to create a sustainable economy and diversity of employment opportunities by:

- Retaining areas identified as *Core Industrial and Employment Areas* in the Strategic Framework Plan at Clause 02.04 as areas for industry and employment.
- Maintaining industry and employment uses in areas identified as *Employment Areas* and potentially transition to a broader business base that contributes to economic regeneration and more diverse employment opportunities.
- Supporting the economic viability and growth of activity centres.
- Supporting creative industries, entrepreneurs and associated activities, including education and training, live music venues, performance, studios and coworking spaces, in Activity Centres, the Brunswick Design District and Core Industrial and Employment Areas as shown on the Strategic Framework Plans *Economic Development* and *Activity Centres* at Clause 2.04.

Economic Development Framework Plan

Hume City Council



0 1 2 3 4 Kilometers

14/05/2020
C164pt1moreAmore

SCHEDULE 18 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO18**.

BRUNSWICK ACTIVITY CENTRE - SYDNEY ROAD AND UPFIELD CORRIDOR

1.0

14/01/2021
C167more

Design objectives

- To encourage a new mid-rise built form character with lower built form at the interfaces with the adjoining low rise residential areas.
- To complement the valued built form and heritage character along Sydney Road and respect the form, design and context of buildings of individual heritage significance in the precinct.
- To ensure the street wall remains the visually dominant element of all development in Sydney Road and that any height above the street wall is visually recessive, subservient and does not dominate the streetscape appearance.
- To establish a new cohesive built form character in off-corridor locations to the east and west of Sydney Road to achieve an appropriate balance between a sense of enclosure and openness and to ensure new street walls reinforce the existing character of street walls in nominated off-corridor streets.
- To protect the amenity of existing and proposed public open spaces and key pedestrian streets, and maintain reasonable amenity for residential properties within and adjacent to the activity centre.

2.0

14/01/2021
C167more

Buildings and works

A permit is not required to:

- Install an automatic teller machine.
- Alter an existing building facade provided that:
 - The alteration does not include the installation of an external roller shutter;
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
- Construct or extend one dwelling on a lot of more than 300 square metres. This exemption does not apply to:
 - Construct a dwelling if there is at least one dwelling existing on the lot;
 - Construct or extend a dwelling if there are two or more dwellings on the lot;
 - Construct or extend a dwelling if it is on common property; and
 - Construct or extend a front fence within 3 metres of a street if the fence is associated with 1 or more dwellings on a lot or a residential building, and the fence exceeds the maximum height specified in Clause 55.06-2.
- Construct or carry out works normal to a dwelling.
- Construct or extend an outbuilding (other than a garage or carport) on a lot provided the gross floor area of the outbuilding does not exceed 10 square metres and the maximum building height is not more than 3 metres above ground level.

Building height

- Building height should not exceed the preferred maximum building heights as shown in *Overall Building and Streetwall Heights, Map 1A (North) and Map 1B (South)*.

- Building height is the vertical distance between the footpath or natural surface level at the centre of the site frontage and the highest point of the building. This does not include architectural features and service equipment including plant rooms, lift overruns, structures associated with green roof areas and other such equipment provided that the following criteria are met:
 - No more than 50% of the roof area is occupied by the equipment (other than solar panels);
 - The equipment is located in a position on the roof so as to minimise additional overshadowing on neighbouring properties and public spaces;
 - The equipment does not extend higher than 3.6 metres above the maximum building height; and
- The equipment and screening is integrated into the design of the building to the satisfaction of the responsible authority.

Buildings of individual heritage significance

- Development on sites containing buildings of individual heritage significance where no preferred maximum height is specified on *Map 1A (North)* and *Map 1B (South)* should demonstrate:
 - How the proposed building height responds to the heritage significance of the site and its context; and
 - How the design objectives of this schedule have been met.

Street walls

- Buildings within the commercial zone and mixed use zone should be constructed to the street frontage.
- Street wall height on the following streets should be between 5 and 8 metres:
 - Barkly Street (west of Black Street);
 - Union Street;
 - Manallack Street, Railway Place (north of Union Street);
 - Charles Street; and
 - Thomas Street.
- Street wall heights on the following streets should be between 7 and 10 metres:
 - Little Gold Street;
 - Florence Street;
 - Rosser Street;
 - Frith Street;
 - Beith Street;
 - McIver Street;
 - Howarth Street;
 - Wilkinson Street
 - Merri Street; and
 - Saxon Street.

MORELAND PLANNING SCHEME

- Street wall height on Sydney Road (including parapets) must be between 8 and 11 metres. A permit cannot be granted to construct a building or carry out works which are not in accordance with this requirement. This does not include
 - Land on Sydney Road south of Brunswick Road, where the street wall height (including parapets) should be between 8 and 11 metres;
 - Alterations to an existing façade provided the existing street wall height does not change.
- Street wall height on the following street should be between 8 and 11 metres:
 - Barkly Street (between Sydney Road and Black Street);
 - Dawson Street;
 - Victoria Street; and
 - Albert Street (east of Sydney Road).
- Street wall heights on the following streets should be between 9 and 12 metres:
 - Albion Street;
 - Breese Street;
 - West Street;
 - Duckett Street;
 - Hope Street;
 - Ovens Street;
 - Merrifield Street;
 - Ballarat Street;
 - Phoenix Street between Sydney Road and the Upfield Rail Line; and
 - Lobb Street.
- Street wall heights on Albert Street (west of Sydney Road) should be between 11 and 14 metres.
- Street wall heights on Dods Street should be between 12 and 15 metres.
- Street wall heights on Wilson Avenue should be between 15 and 18 metres.
- Street wall heights on the following streets should be between 17 and 20 metres:
 - Brunswick Road;
 - Michael Street between Sydney Road and Upfield Rail Line;
 - Weston Street;
 - Staley Street;
 - David Street (West side); and
 - Barkly Street (east of Sydney Road).
- On secondary street frontages of corner properties having the primary frontage to Sydney Road, the street wall height requirements on the Sydney Road frontage also apply to the secondary street frontage for the depth of the upper level setback.
- On the following corner properties, the street wall height at the frontage is to apply to the sideage for the depth of the upper level setback:
 - Black Street and Barkly Street;

- Black Street and Wilson Avenue;
 - Saxon Street and Michael Street;
 - Saxon Street and Phoenix Street;
 - Henry Street and Albert Street;
 - Breese Street and Victoria Street;
 - Boase Street and Ballarat Street;
 - Boase Street and William Street;
 - Staley Street and Blyth Street (south of Blyth);
 - Frith Street and McIver Street;
 - Frith Street and Howarth Street; and
 - Beith Street and Howarth Street.
- Street facades within the Commercial 1 Zone should extend to both sides of the property boundary.
 - Development that seeks to vary the street wall heights must demonstrate how the design objectives and requirements of this schedule will be met.

Buildings of individual heritage significance

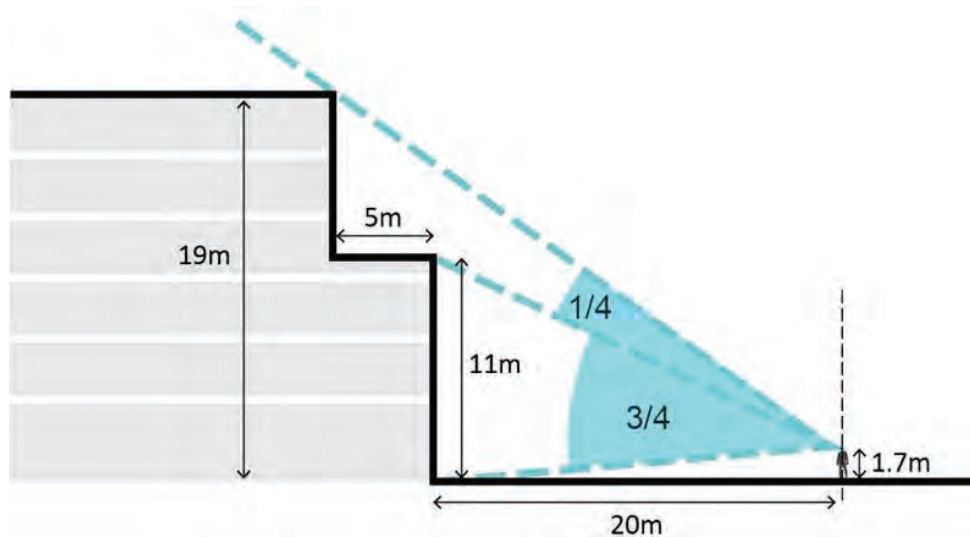
- Development on sites containing buildings of individual heritage significance where no preferred maximum street wall height is specified on *Map 1A (North)* and *Map 1B (South)* should demonstrate:
 - Whether a street wall is appropriate having regard to heritage significance of the site and its context;
 - How the design objectives of this schedule have been met.

Upper levels (Sydney Road)

- Any part of a building (including balconies but excluding shading devices) above the street wall height should:
 - Be set back at least 5 metres from the street boundary.
 - On narrow corner lots the upper level setback at the secondary street frontage may be varied. Narrow lots are generally those lots where the depth is greater than five times the width, and where the lot size is smaller than 750m².
 - Be designed to ensure that it occupies no more than one quarter of the vertical angle defined by the whole building in the view from an eye-level of 1.7 metres on the opposite side of the street, as illustrated in *Figure 1* below.
 - Where an existing building with a street wall height of less than 11 metres is to be retained for heritage reasons new development may occupy more than one quarter of the vertical angle defined by the whole building outlined in *Figure 1* below.
 - Adopt the same street setback for at least 75% of the height of the upper levels to avoid 'wedding cake' built form outcomes.
 - Materials at the upper levels of the building should be distinct from materials of the lower levels of the building.

- The architectural expression of the upper and lower building components should be complementary and upper levels should be visually recessive.
- Be designed to respect the form and design of adjacent civic buildings and heritage places.

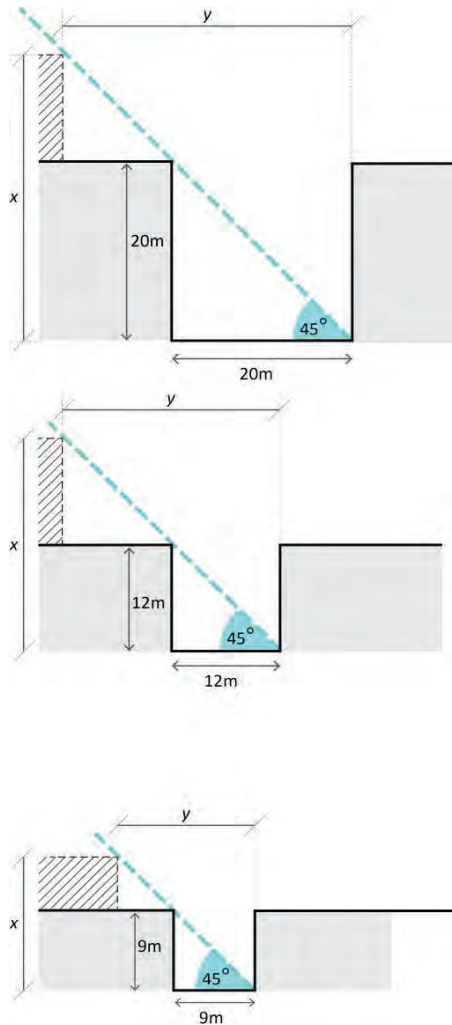
Figure 1: Upper Level Setbacks along Sydney



Upper levels (streets other than Sydney Road)

- Any part of a building above the street wall height should:
 - Be setback at least 5 metres from the street boundary. Balconies and other architectural features may protrude into the setback by a maximum of 2 metres.
 - On narrow corner lots the upper level setback at the secondary street frontage may be varied. Narrow lots are generally those lots where the depth is greater than five times the width, and where the lot size is smaller than 750m².
 - From ground level not exceed the horizontal distance from the opposite street boundary as illustrated in *Figure 2* below.
 - Adopt the same street setback for at least 75% of the height of the upper levels to avoid 'wedding cake' built form outcomes.
 - Be designed to respect the form and design of adjacent civic buildings and heritage places.
- Development that seeks to vary the upper level setbacks must demonstrate how the design objectives and requirements of this schedule will be met.

Figure 2: Upper level setbacks and heights (streets other than Sydney Road)



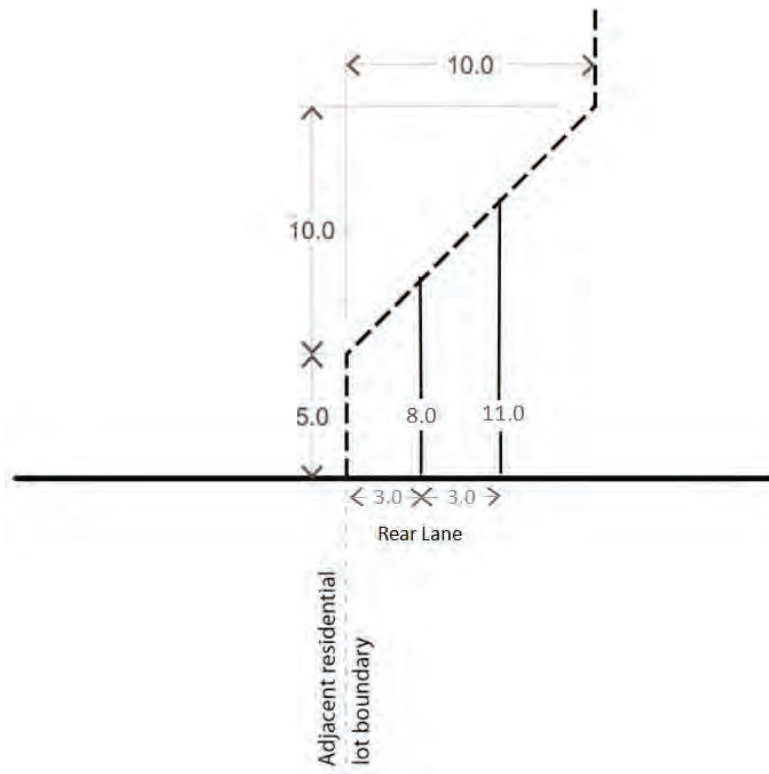
Buildings of individual heritage significance

- Development on sites containing buildings of individual heritage significance where no upper level setback is specified should demonstrate:
 - How the proposed upper level setback responds to the heritage significance of the site and its context; and
 - How the design objectives of this schedule have been met.

Setbacks to residential land

- Any part of a building adjacent to residentially zoned land outside the activity centre (including across a lane) should be set back from the residential boundary at that interface by a dimension equivalent to its height above 5 metres, up to a maximum setback of 10 metres, as illustrated in *Figure 3* below. This equates to:
 - A maximum height of 5 metres at the boundary of the adjacent residential property.
 - A maximum height of 8 metres where the site is separated from the adjacent residential property by a 3 metre wide lane.
 - A maximum height of 11 metres where existing lanes are widened to 6 metres.

Figure 3: Setbacks adjacent to residentially zoned land outside the activity centre



Building layout and detailed design

- Development should incorporate active edges at street level fronting all streets (except rear access lanes) and the Upfield Shared Path in accordance with *Building Edge Conditions Map 3A (North) and Map 3B (South)* to invigorate street life and contribute to a safe and pedestrian friendly environment. The Frontage types in *Map 3A (North) and Map 3B (South)* are as follows:
 - Frontage Type A: Retail – A display window and/or entrance, measuring 65 - 80% of the width of the street frontage of each individual premise.
 - Frontage Type B: Commercial – A display window and/or entrance, measuring at least 60% of the width of the street frontage of each individual premise.
 - Frontage Type C: Residential – Individual entry doors to ground floor dwellings. Privacy is to be provided by elevating the ground floor approximately 0.5 to 1 metre above the street level and/or provide a landscaped front setback.
- Development that abuts the Upfield Shared Path should be designed to avoid large areas of blank walls. Where unavoidable, large areas of blank walls should be treated with materials, artwork or in other ways that provide an attractive and visually interesting environment.
- Development should incorporate windows in all upper level facades facing streets and the Upfield Shared Path.
- Development should minimise the impact of vehicle access and car parking on the public realm by:
 - Locating vehicle access at the rear of buildings where possible;
 - On secondary frontages, rear access is preferred but where not possible, vehicle access should be provided to the side of corner properties away from the front façade; and
 - Locating car parking where it cannot be seen from the public realm.
- In commercial and mixed use zones, development should have a ground floor ceiling height that allows for a range of commercial uses.

- Development should incorporate awnings over the footpath for the full width of the building frontage along Sydney Road and Key Pedestrian Streets indicatively shown on *Open Space and Public Realm Network Map 2A (North) and Map 2B (South)*.
- Services cabinets should be located at the rear of buildings wherever possible, or the side of corner buildings away from the front façade where rear access is not available. Where they can only be located in the front façade, the size of services cabinets should be minimised and they should be integrated with the overall façade design.
- Building facades on the street boundaries of Sydney Road should incorporate vertical articulation to reinforce the prevailing fine-grain pattern of subdivision and buildings.

Public realm

- Development adjacent to Jewell, Brunswick and Anstey Railway Stations should maintain or enhance the visibility of and access to railway station buildings.
- Development should incorporate the proposed public linkages indicatively shown on *Map 2A (North) and 2B (South)*.
- Development should maximise permeability through the Activity Centre and the accessibility to the Upfield Shared Path.
- Development should not overshadow the opposite footpath of Sydney Road and Key Pedestrian Streets indicatively shown on *Map 2A (North) and Map 2B (South)* between 10.00am and 2.00pm at the equinox.
- Development should not overshadow more than 50% of any existing or proposed public space, including parks and plazas, between 10.00am and 2.00pm at the equinox.

Upfield shared path interface

- The ground floor of development abutting the Upfield Shared Path should be set back a minimum of 1 metre.

Category 2 Employment Areas

- Development within Category 2 – Employment Areas should achieve commercial built form outcomes, including increased floor to ceiling heights and flexible floor plates, at ground and first floor to facilitate opportunities for employment uses.
- Ground floor commercial spaces should provide for activation, for example through entrances, transparent glazing and architectural features which encourage interaction and visual interest.

Exemption from notice and review

An application to construct a building or construct or carry out works is exempt from the notice requirements of Section 52(1) (a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Planning and Environment Act 1987 (the Act) unless the proposal seeks to do any of the following:

- Exceed the preferred maximum building height specified in section 2.0 of this schedule, or
- Exceed the preferred maximum street wall height specified in section 2.0 of this schedule, or
- Reduce the setbacks above the street wall specified in section 2.0 of this schedule, or
- Reduce the rear setback requirements specified in section 2.0 of this schedule.

3.0

14/05/2020
C164pt1moreAmore

Subdivision

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52 (1) (a), (b) and (d), the decision requirements of Section 64 (1), (2) and (3) and the review rights of Section 82 (1) of the Act.

4.0

14/05/2020
C164pt1moreAmore

Advertising signs

None specified.

5.0

~~14/01/2021~~
Proposed
C212

Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- For a development of four or more storeys:
 - A computer model of the proposal compatible with Council's software;
 - A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development;
 - Streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape elevation;
 - Three-dimensional perspective diagrams which show the form of the proposal at oblique views from neighbouring streets where any part of the proposal will be visible.

6.0

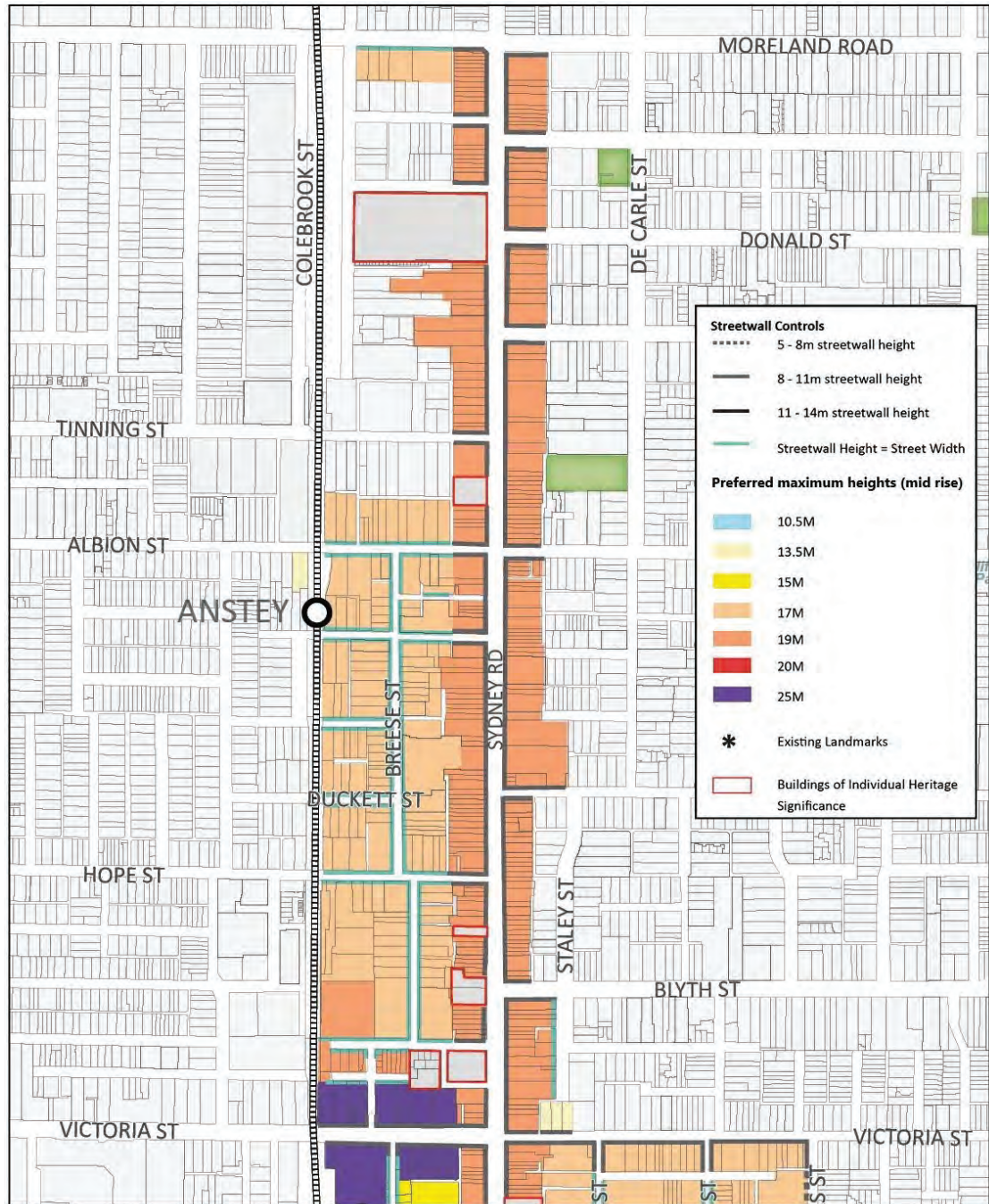
~~14/01/2021~~
~~14/01/2021~~
C467more
Proposed
C212

Decision guidelines

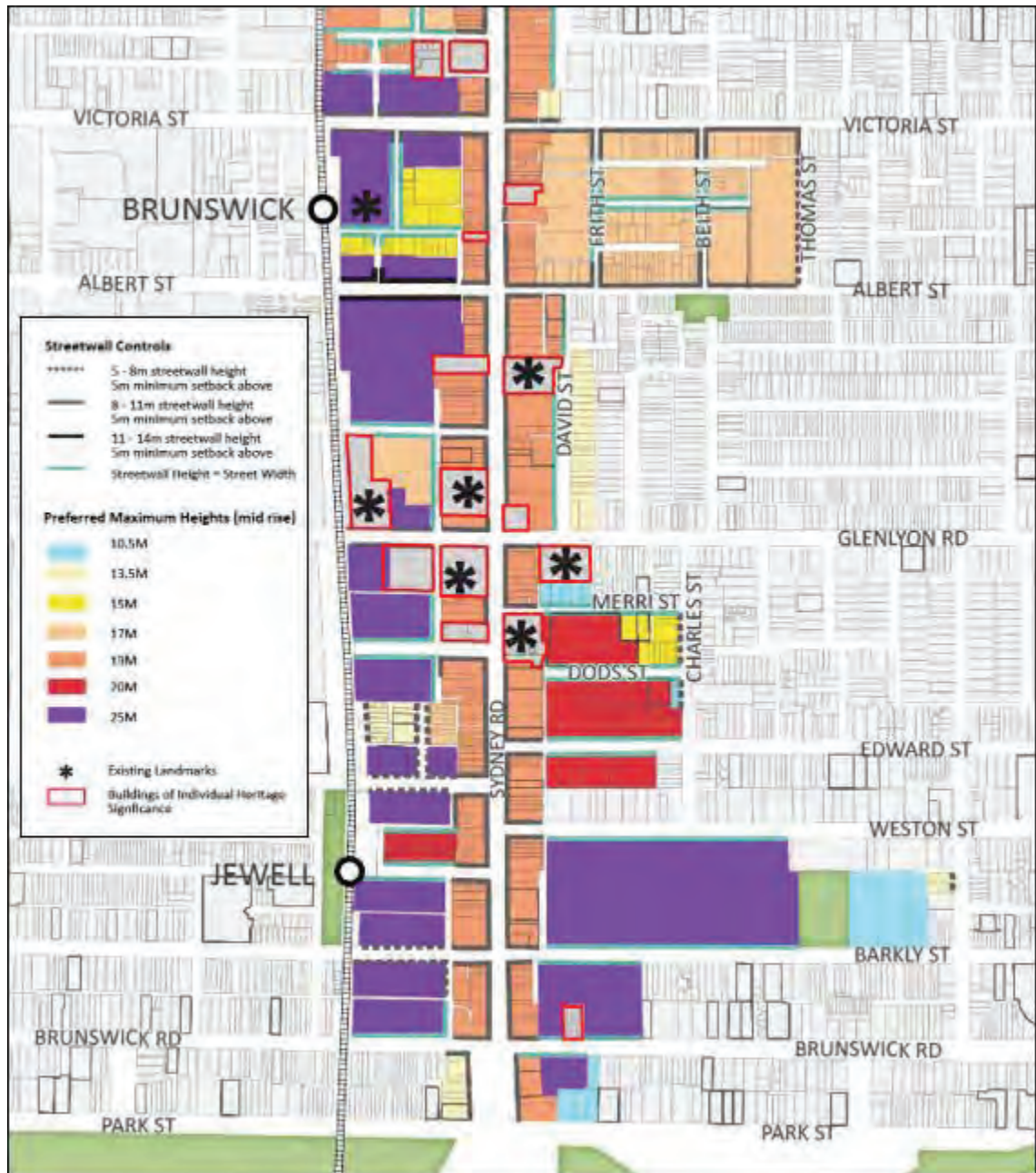
The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The *Brunswick Structure Plan Reference Document, Moreland City Council, 2018*.
- How development within Category 2 Employment areas responds to the built form requirements for Category 2 Employment areas.
- The opportunities and constraints of the site.
- Whether the maximum building height is achievable having regard to lot size, dimensions (width and depth) and/or those sites within or adjoining the Heritage Overlay.
- Whether an increased upper level setback is required having regard to the heritage significance of the site or an adjoining site within the Heritage Overlay.
- Whether the preferred maximum street wall height is achievable having regard to the heritage significance of the site or an adjoining site within the Heritage Overlay.
- How the development responds to the fine grain character of Sydney Road.
- How the development considers views along Sydney Road to landmark buildings of individual heritage significance.
- The extent to which development achieves the design objectives in Part 1.0 of this schedule.

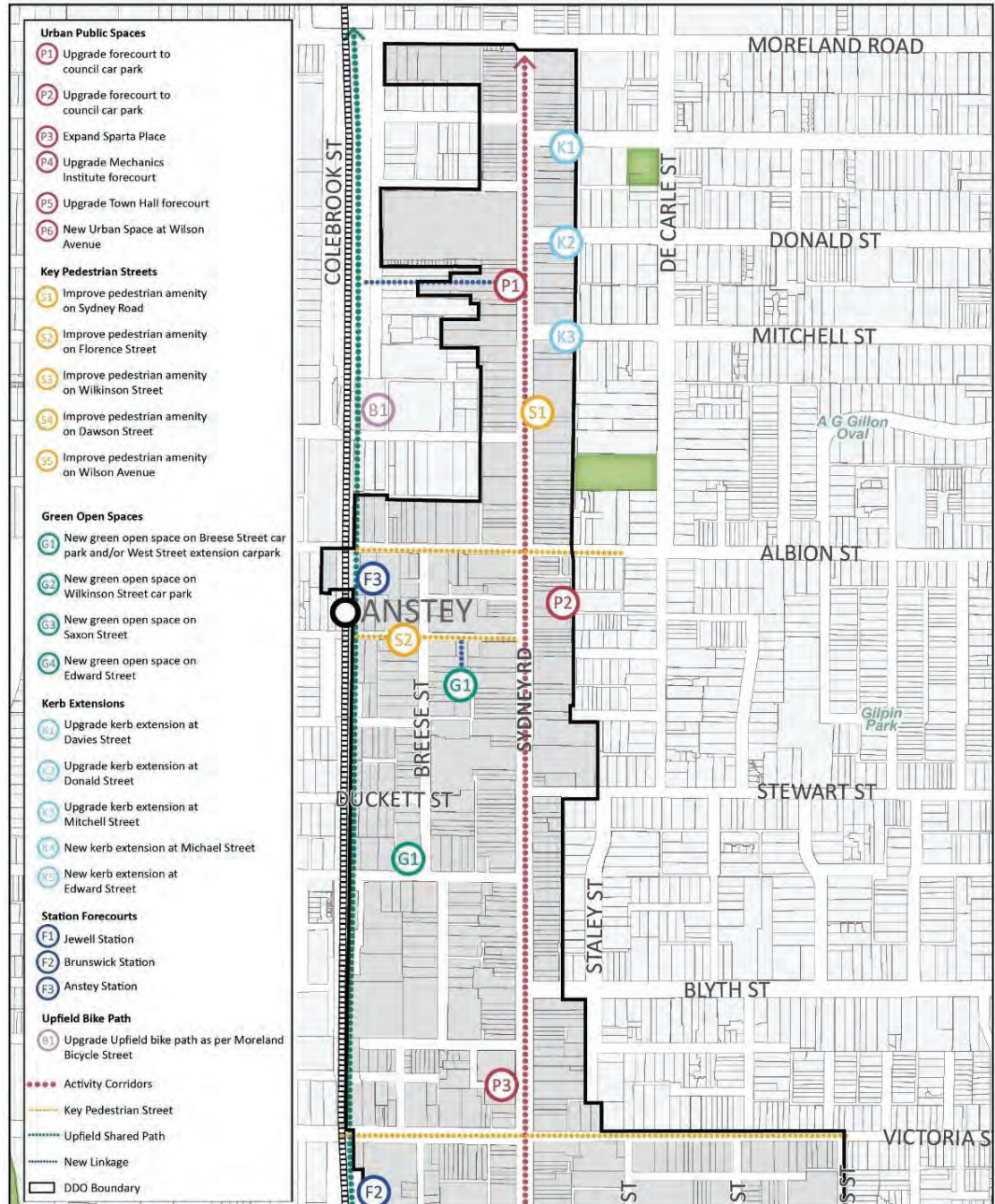
Map 1A: Overall Building and Streetwall Heights (North)



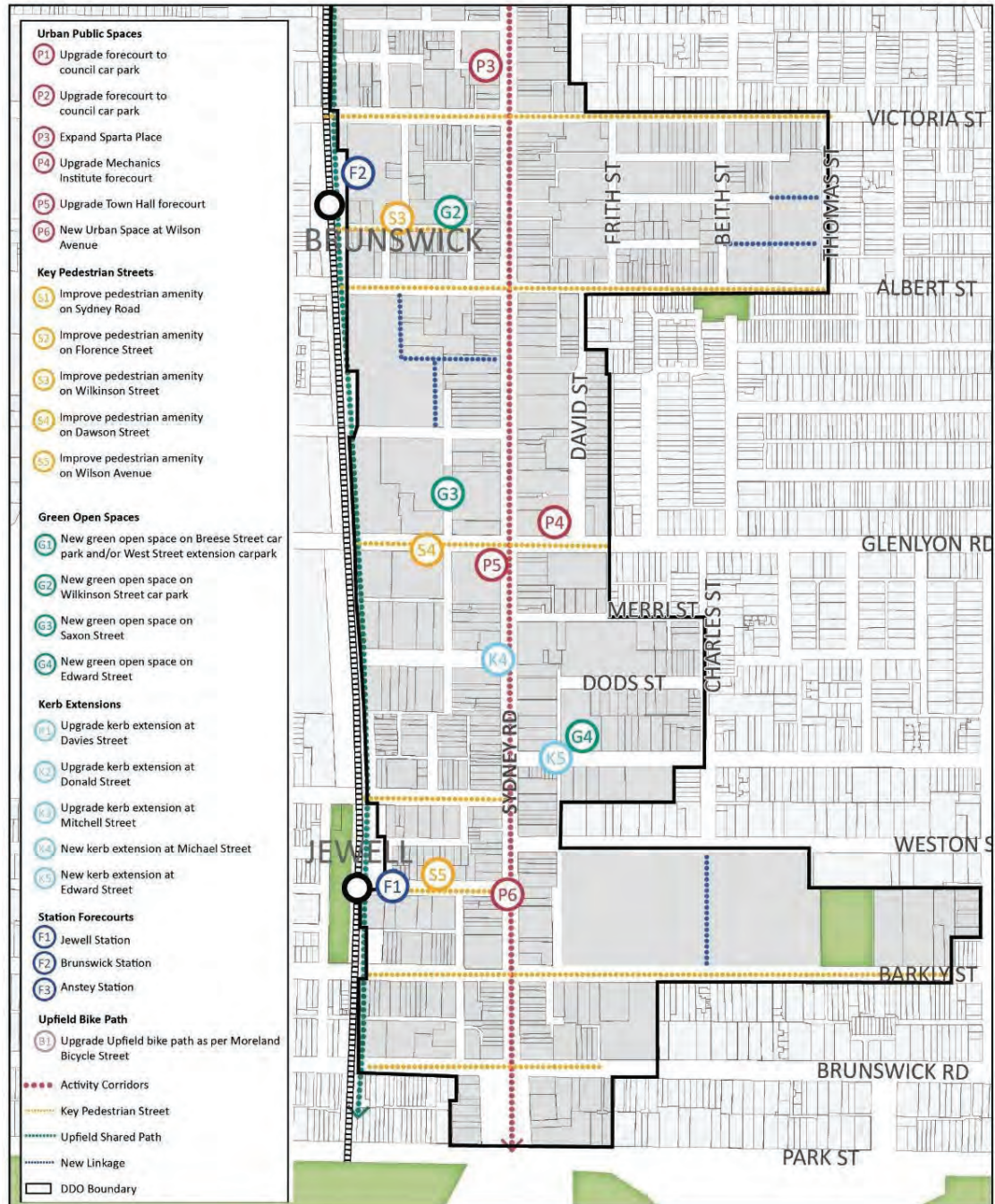
Map 1B: Overall Building and Streetwall Heights (South)



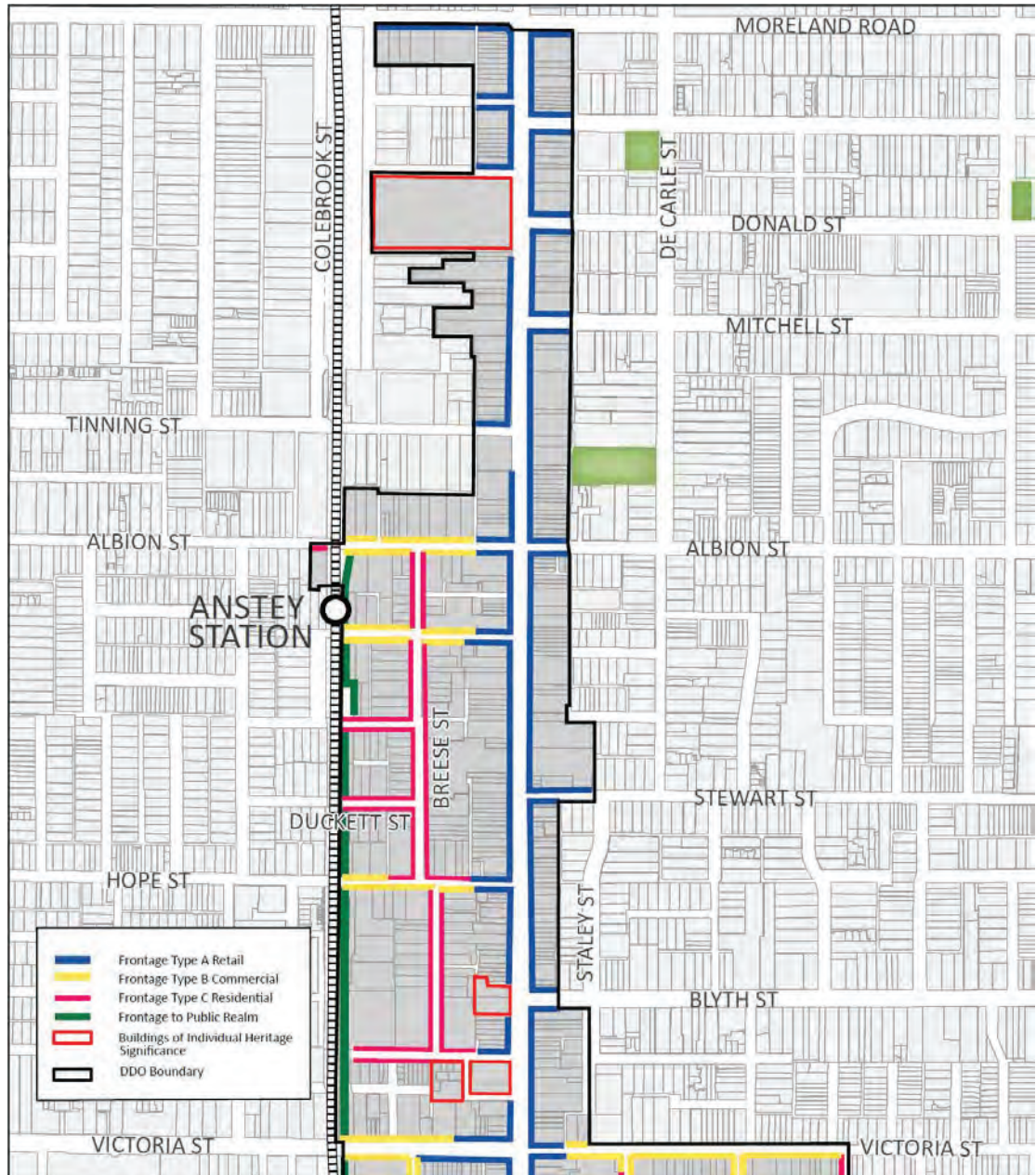
Map 2A: Open Space and Public Realm Network (North)



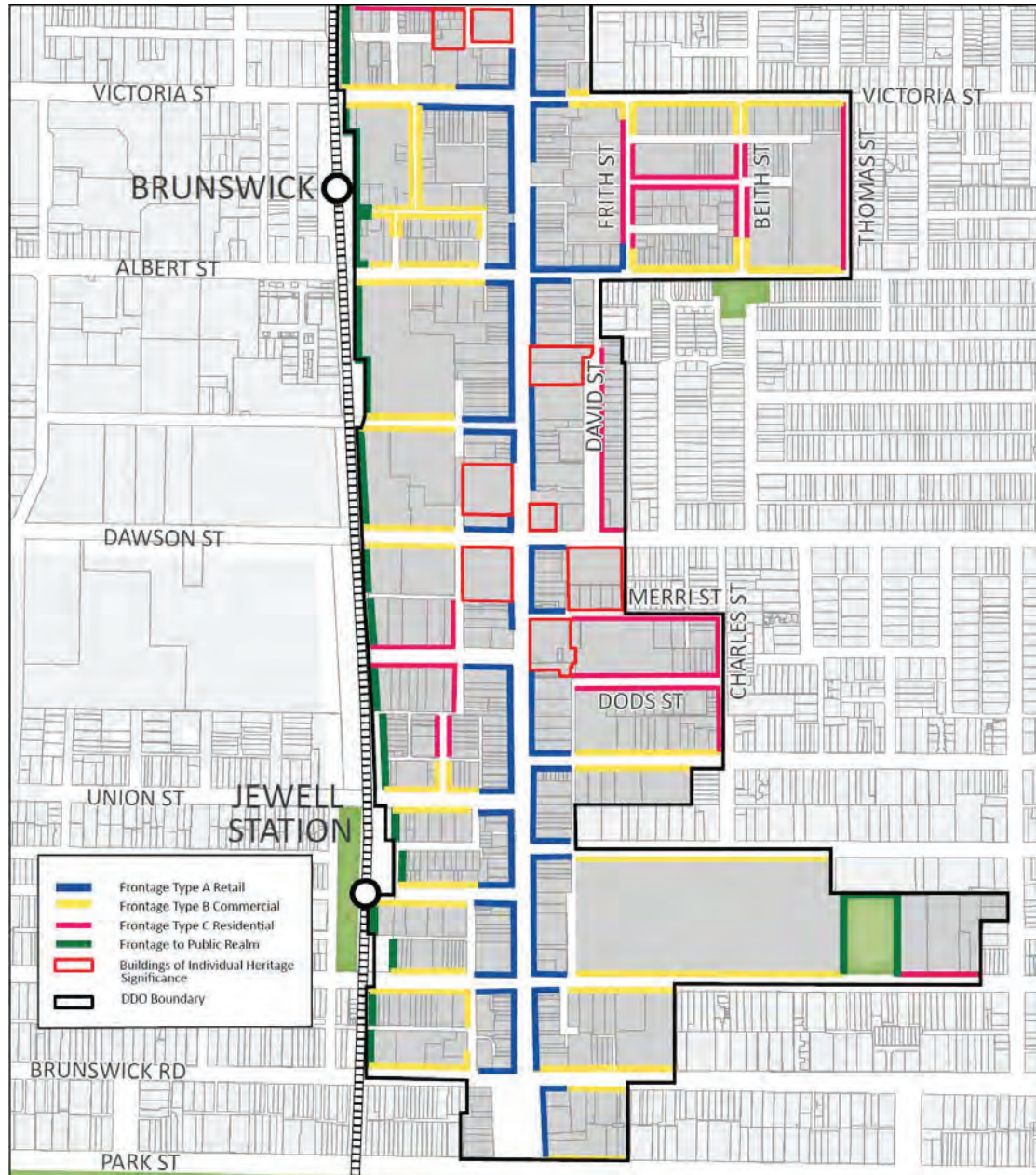
Map 2B: Open Space and Public Realm Network (South)



Map 3A: Building Edge Conditions (North)



Map 3B: Building Edge Conditions (South)



14/05/2020
C164pt1moreAmore

SCHEDULE 19 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO19**.

BRUNSWICK ACTIVITY CENTRE – LYGON STREET LOCAL AREA

1.0

14/05/2020
C164pt1moreAmore

Design objectives

- To create a new mid rise built form character that provides a built form transition between the Lygon Activity Corridor and adjoining low-rise residential areas.
- To ensure highly visible development is limited to identified key redevelopment sites and responds to specific design objectives.
- To ensure the street wall remains the visually dominant element of all development in Lygon Street and that any height above the street wall is visually recessive, subservient and does not dominate the streetscape appearance.
- To ensure development is designed to respect the form, design and context of buildings of individual heritage significance.
- To protect and enhance the amenity, and maintain solar access to existing and proposed public open spaces and key pedestrian streets, and maintain reasonable amenity for residential properties adjacent to or within the activity centre.

2.0

14/01/2021
C167more

Buildings and works

A permit is not required to:

- Install an automatic teller machine.
- Alter an existing building facade provided that:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
- Construct or extend one dwelling on a lot of more than 300 square metres. This exemption does not apply to:
 - Construct a dwelling if there is at least one dwelling existing on the lot.
 - Construct or extend a dwelling if there are two or more dwellings on the lot.
 - Construct or extend a dwelling if it is on common property.
 - Construct or extend a front fence within 3 metres of a street if the fence is associated with 1 or more dwellings on a lot or a residential building, and the fence exceeds the maximum height specified in Clause 55.06-2.
- Construct or carry out works normal to a dwelling.
- Construct or extend an outbuilding (other than a garage or carport) on a lot provided the gross floor area of the outbuilding does not exceed 10 square metres and the maximum building height is not more than 3 metres above ground level.

Building height

- Building height should not exceed the preferred maximum building heights as shown in *Lygon Street Local Area Built Form Controls, Map 1A (North)* and *Map 1B (South)*.
- Building height is the vertical distance between the footpath or natural surface level at the centre of the site frontage and the highest point of the building. This does not include architectural features and service equipment including plant rooms, lift overruns, structures associated with green roof areas and other such equipment provided that the following criteria are met:
 - Not more than 50% of the roof area is occupied by the equipment (other than solar panels);
 - The equipment is located in a position on the roof so as to minimise additional overshadowing or neighbouring properties and public spaces;
 - The equipment does not extend higher than 3.6 metres above the maximum building height; and
 - The equipment and screening is integrated into the design of the building to the satisfaction of the responsible authority.

Buildings of individual heritage significance

- Development on sites containing buildings of individual heritage significance shown on *Map 1A (North)* and *Map 1B (South)* should demonstrate:
 - How the proposed building height responds to the heritage significance of the site and its context; and
 - How the design objectives have been met.

Street walls

- Street wall heights should not exceed the street wall heights specified on *Map 1A (North)*, *Map 1B (South)* and *Diagram 1: Lygon Street Local Area Section Diagrams*.
- Development on sites containing buildings of individual heritage significance shown on *Map 1A (North)* and *Map 1B (South)* where no preferred maximum street wall height is specified should demonstrate:
 - Whether a street wall is appropriate having regard to the heritage significance of the site and its context;
 - How the proposed street wall height responds to the heritage significance of the site and its context; and
 - How the design objectives have been met.
- Development that seeks to vary the upper storey setbacks detailed in *Map 1A (North)*, *Map 1B (South)* and *Diagram 1* should demonstrate how the design objectives and requirements of this schedule will be met.

Upper levels

- Development above the street wall height or behind an existing parapet should be:
 - Set back at least the minimum distance specified in *Map 1A (North)*, *Map 1B (South)* and *Diagram 1*. Balconies must not intrude into this setback.
 - Designed to differentiate it from the building's street wall component through façade articulation and materials.
- Material at the upper levels of buildings should be distinct from materials of the levels of the building.

- The architectural expression of the upper levels and lower level building components should be complementary and upper levels should be visually recessive.
- Development that seeks to reduce the upper level setback specified on *Map 1A (North)*, *Map 1B (South)* and *Diagram 1* should demonstrate how the design objectives will be met.
- On narrow corner lots the upper level setback at the secondary street frontage may be varied. Narrow lots are generally those lots where the depth is greater than five times the width, and where the lot size is smaller than 750m².

Buildings of individual heritage significance

- Development on sites containing buildings of individual heritage significance shown on *Map 1A (North)*, *Map 1B (South)* where no preferred maximum upper level setback is specified should demonstrate:
 - How the proposed upper level setback responds to the heritage significance of the site and its context; and
 - How the design objectives have been met.

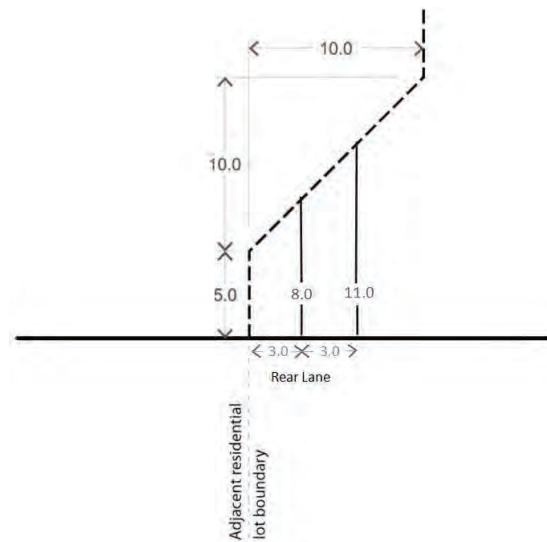
Key redevelopment sites

- Development on key redevelopment sites identified in Map 1 should:
 - Ensure new development contributes to the overall legibility of the Lygon Street Local Area and Brunswick Activity Centre as a whole by defining a high quality gateway at the northern end of Lygon Street.
 - Enhance the character of the precinct with a new high quality, highly visible and accessible open space adjacent the intersection of Lygon and Albion streets.
 - Enhance the view corridor from Holmes Street on approach from the north and the view corridor from Lygon Street on approach from the south.
 - Ensure new development does not dominate the Lyndhurst Hotel.
 - Protect identified heritage places to the east side of Lygon Street.
 - Integrate high quality public open space into the redevelopment of the property at 1 Holmes Street.

Setbacks to residential land

- Any part of a building adjacent to residentially zoned land outside the activity centre (including across a lane) should be set back from the residential boundary at that interface by a dimension equivalent to its height above 5 metres, up to a maximum setback of 10 metres as shown in *Figure 1* below. This equates to:
 - A maximum height of 5 metres at the common boundary.
 - A maximum height of 8 metres where the site is separated from the adjacent residential property by a 3 metre wide lane.
 - A maximum height of 11 metres where existing lanes are widened to 6 metres.

Figure 1: Setbacks adjacent to residentially zoned land outside the activity centre



Building layout and detailed design

- Development should incorporate active edges at street level fronting all streets (except rear access lanes) in accordance with *Figure 6: Land use pattern and activity – Lygon Street corridor* as shown in the *Brunswick Structure Plan Reference Document, Moreland City Council, 2018*, to invigorate street life and contribute to a safe and pedestrian friendly environment. The frontage types are as follows:
 - Frontage Type A: Retail - A display window and/or entrance, measuring at least 65% - 80% of the width of the street frontage of each individual premises.
 - Frontage Type B: Commercial - A display window and/or entrance, measuring at least 60% of the width of the street frontage of each individual premise.
 - Frontage Type C: Residential - Individual entry doors to ground floor dwellings. Privacy is to be provided by elevating the ground floor approximately 0.5 to 1 metre above the street level and/or provide a landscaped front setback.
- Development should incorporate windows in all upper level facades facing streets.
- Development should minimise the impact of vehicle access and car parking on the public realm by:
 - Locating vehicle access at the rear of buildings where possible;
 - On secondary frontages, rear access is preferred but where vehicle access is not possible should be provided to the side of corner properties away from the front facade; and
 - Locating car parking where it cannot be seen from the public realm.
- In commercial and mixed use zones, development should have a ground floor ceiling height that allows for a range of appropriate uses.
- In commercial zones, development should incorporate awnings over the footpath for the full width of the building frontage.
- Services cabinets should be located at the rear of buildings wherever possible, or the side of corner buildings away from the front façade where rear access is not available. Where they can only be located in the front façade, the size of services cabinets should be minimised and they should be integrated with the overall façade design.
- Building facades on the street boundaries of Lygon Street should incorporate vertical articulation to reinforce the fine-grain the prevailing pattern of subdivision and buildings.

Category 2 Employment Areas

- Development within Category 2 – Employment Areas should achieve commercial built form outcomes, including increased floor to ceiling heights and flexible floor plates, at ground and first floor to facilitate opportunities for employment uses.
- Ground floor commercial spaces should provide for activation, for example through entrances, transparent glazing and architectural features which encourage interaction and visual interest.

Exemption from notice and review

An application to construct a building or construct or carry out works is exempt from the notice requirements of Section 52(1) (a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the *Planning and Environment Act 1987* (the Act) unless the proposal seeks to do any of the following:

- Exceed the preferred maximum building height specified in section 2.0 of this schedule, or
- Exceed the preferred maximum street wall height specified in section 2.0 of this schedule, or
- Reduce the setbacks above the street wall specified in section 2.0 of this schedule, or
- Reduce the rear setback requirements specified in section 2.0 of this schedule.

3.0

14/05/2020
C164pt1moreAmore

Subdivision

Subdivision of existing sites not associated with a development proposal that achieves the design objectives and requirements of this schedule is discouraged.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0

14/05/2020 - /- /20-
C164pt1moreAmore
Proposed C212

Advertising Signs

None specified.

5.0

- /- /20-
Proposed
C212

Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- For a development of four or more storeys:
 - A computer model of the proposal compatible with Council's software;
 - A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development;
 - Streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape elevation;
 - Three-dimensional perspective diagrams which show the form of the proposal at oblique views from neighbouring streets where any part of the proposal will be visible.

5.0

14/04/2021
- /- /20-
C167more
Proposed
C212

Decision guidelines:

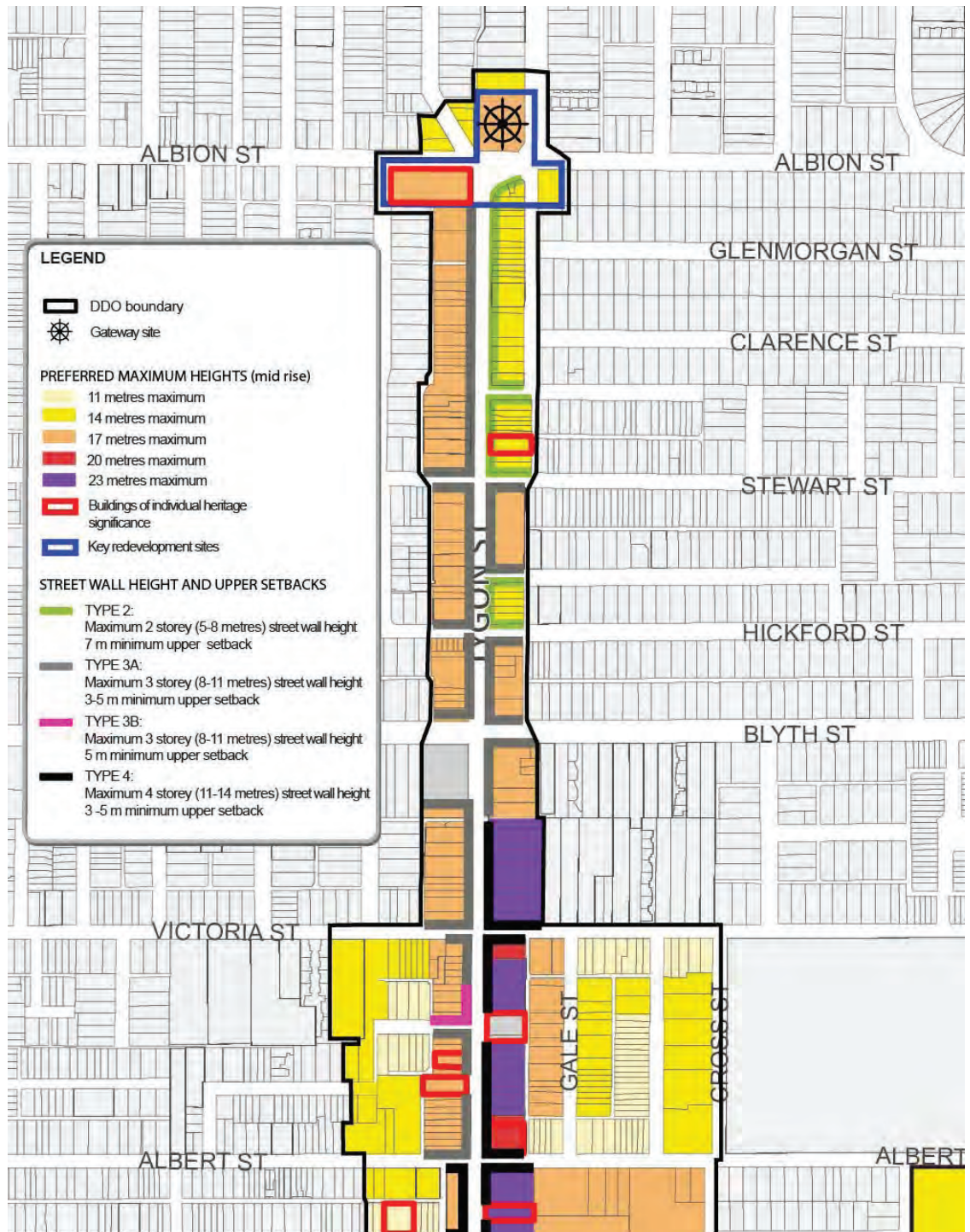
Before deciding on an application, the responsible authority must consider, as appropriate:

- The *Brunswick Structure Plan Reference Document, Moreland City Council, 2018*.
- How development within Category 2 Employment areas responds to the built form requirements for Category 2 Employment areas.
- The opportunities and constraints of the site.

MORELAND PLANNING SCHEME

- How the development responds to the fine grain character of Lygon Street.
- How the development considers views along Lygon Street to buildings of individual significance.
- Whether the maximum building height is achievable having regard to the heritage significance of the site or an adjoining site within the Heritage Overlay.
- Whether an increased upper level setback is appropriate having regard to the heritage significance of the site or an adjoining site within the Heritage Overlay.

Map 1A: Lygon Street Local Area Built Form Controls (North)



Map 1B: Lygon Street Local Area Built Form Controls (South)

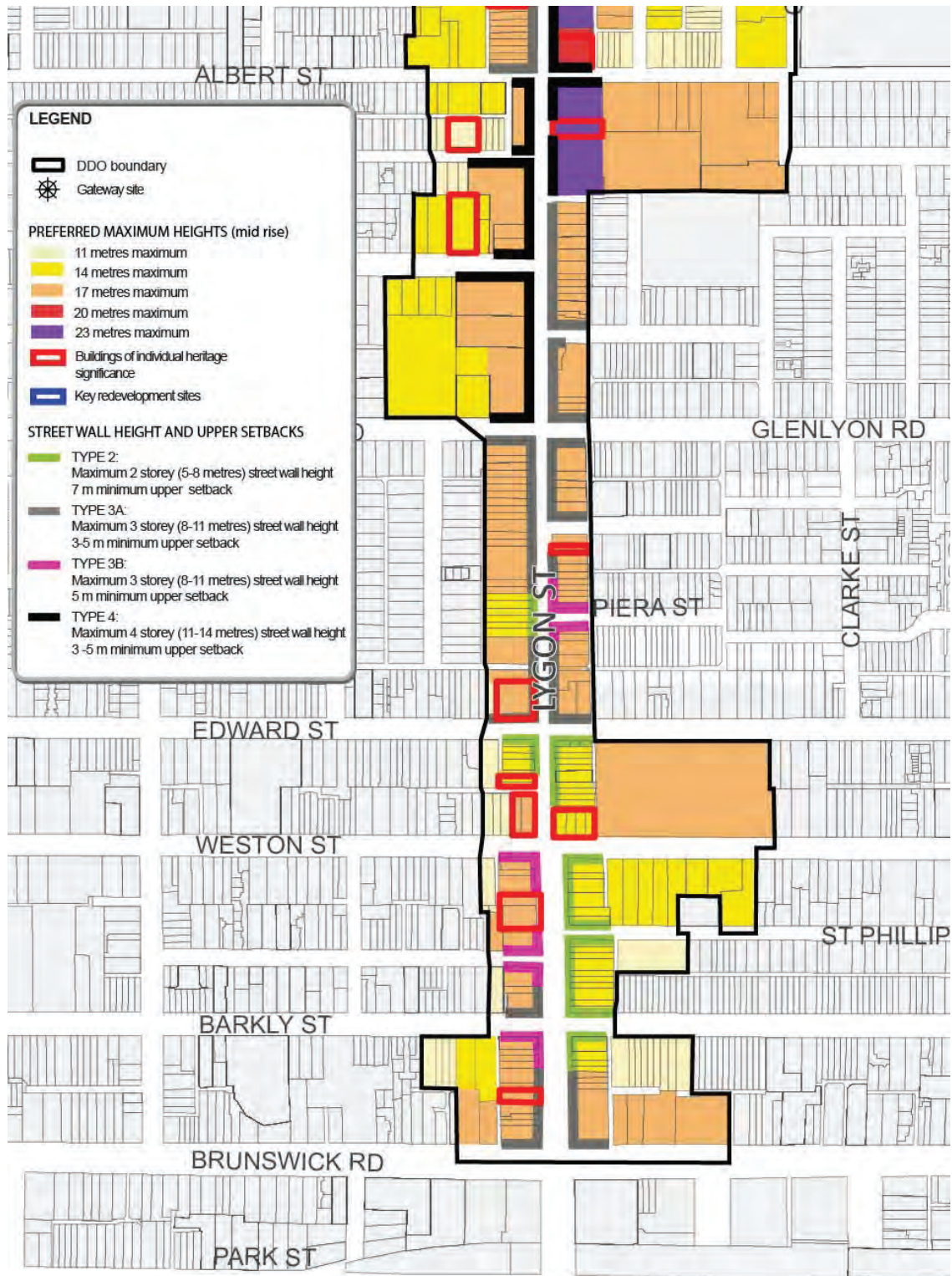
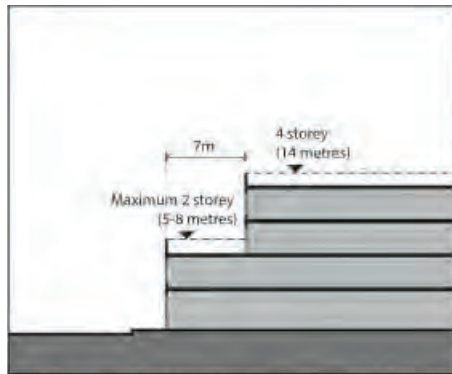
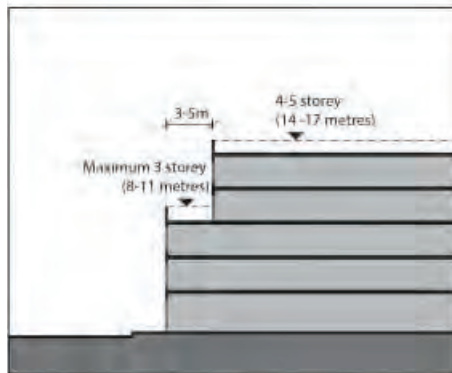


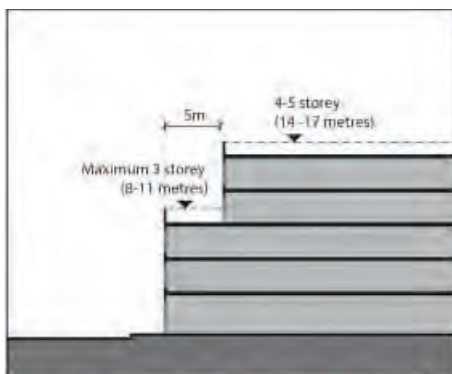
Diagram 1: Lygon Street Local Area Section Diagrams (South)



TYPE 2: Maximum 2 storey (5-8 metres) street wall height
7 m minimum upper setback



TYPE 3A: Maximum 3 storey (8-11 metres) street wall height
3-5 m minimum upper setback



TYPE 3B: Maximum 3 storey (8-11 metres) street wall height
5 m minimum upper setback



TYPE 4: Maximum 4 storey (11-14 metres) street wall height
3-5 m minimum upper setback

14/05/2020
C164pt1moreAmore

SCHEDULE 20 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO** .

BRUNSWICK ACTIVITY CENTRE – NICHOLSON STREET LOCAL AREA

1.0

14/05/2020
C164pt1moreAmore

Design objectives

- To create a new mid-rise built form character that provides a built form transition from the Nicholson Street Activity Corridor to adjoining low rise residential areas.
- To ensure development does not dominate and obstruct sightlines to existing key churches and iconic sites.
- To establish a consistent street wall along the western side of Nicholson Street between Miller Street and Albert Street.
- To ensure development is designed to respect the form and design of civic buildings and heritage places.
- To protect and enhance the amenity, and maintain solar access to existing public open spaces and key pedestrian streets, and maintain reasonable amenity for residential properties adjacent to and within the activity centre.

2.0

14/01/2021
C167more

Buildings and works

A permit is not required to:

- Construct or carry out works for development on site that forms part of a development plan approved in accordance with Schedule 11 to the Development Plan Overlay (DPO11).
- Install an automatic teller machine.
- Alter an existing building facade provided that:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
- Construct or extend one dwelling on a lot of more than 300 square metres. This exemption does not apply to:
 - Construct a dwelling if there is at least one dwelling existing on the lot.
 - Construct or extend a dwelling if there are two or more dwellings on the lot.
 - Construct or extend a dwelling if it is on common property.
 - Construct or extend a front fence within 3 metres of a street if the fence is associated with 1 or more dwellings on a lot or a residential building, and the fence exceeds the maximum height specified in Clause 55.06-2.
- Construct or carry out works normal to a dwelling.
- Construct or extend an outbuilding (other than a garage or carport) on a lot provided the gross floor area of the outbuilding does not exceed 10 square metres and the maximum building height is not more than 3 metres above ground level.

Building height

- Building height should not exceed the preferred maximum building heights as shown in *Nicholson Street Local Area Built Form Controls, Map 1A (North) and Map 1B (South)*.

- Building height is the vertical distance between the footpath or natural surface level at the centre of the site frontage and the highest point of the building. This does not include architectural features and service equipment including plant rooms, lift overruns, structures associated with green roof areas and other such equipment provided that the following criteria are met:
 - Not more than 50% of the roof area is occupied by the equipment (other than solar panels);
 - The equipment is located in a position on the roof so as to minimise additional overshadowing or neighbouring properties and public spaces;
 - The equipment does not extend higher than 3.6 metres above the maximum building height; and
 - The equipment and screening is integrated into the design of the building to the satisfaction of the responsible authority.
- Development on sites where no maximum building height is detailed in *Map 1A (North)* and *Map 1B (South)* should demonstrate:
 - How the proposed overall building height responds to its context including any identified heritage places; and
 - How the design objectives and requirements of this schedule have been met.
 - The height of any building (including services) on 120, 122 and 124 Nicholson must not impact on the Fresnel one associated with radio transmissions from 221 Nicholson Street, Brunswick East. A permit cannot be granted to vary this requirement.

Street walls

- Street wall heights should not exceed the preferred maximum height specified in *Map 1A (North)*, *Map 1B (South)* and *Diagram 1: Nicholson Street Local Area Section Diagrams*.
- Development that seeks to vary the upper storey setbacks detailed in *Map 1A (North)*, *Map 1B (South)* and *Diagram 1* should demonstrate how the design objectives and requirements of this schedule will be met.

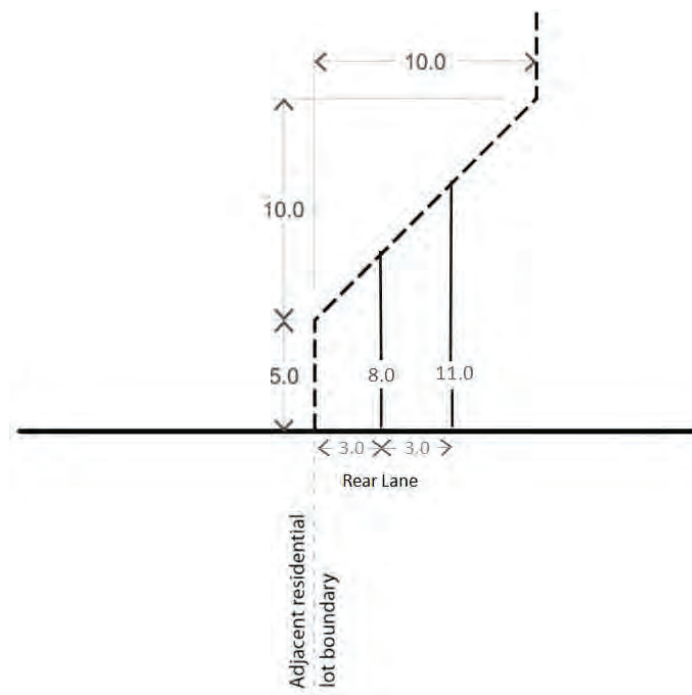
Upper levels

- Development above the street wall height or behind an existing parapet should be:
 - Set back at least the minimum distance specified in *Map 1A (North)*, *Map 1B (South)* and *Diagram 1*. Balconies must not intrude into this setback.
 - Designed to differentiate it from the building's street wall component through façade articulation; and materials.
- Materials at the upper levels of the building should be distinct from materials of the lower levels of the building.
- The architectural expression of the upper level and lower building components should be complementary and upper levels should be visually recessive.
- Development that seeks to reduce the upper level setback specified on *Map 1A (North)*, *Map 1B (South)* and *Diagram 1* should demonstrate how the design objectives will be met.
- On narrow corner lots the upper level setback at the secondary street frontage may be varied. Narrow lots are generally those lots where the depth is greater than five times the width, and where the lot size is smaller than 750m².

Setbacks to residential land

- Any part of a building adjacent to residentially-zoned land outside the activity centre (including across a lane) should be set back from the residential boundary at that interface by a dimension equivalent to its height above 5 metres, up to a maximum setback of 10 metres as shown in Figure 1 below. This equates to:
 - A maximum height of 5 metres at the common boundary.
 - A maximum height of 8 metres where the site is separated from the adjacent residential property by a 3 metre wide lane.
 - A maximum height of 11 metres where existing lanes are widened to 6 metres.

Figure 1: Setbacks adjacent to residentially zoned land outside the activity centre



Buildings layout and detailed design

- Development should incorporate active edges at street level fronting all streets (except rear access lanes) in accordance with *Figure 7: Land use pattern and activity – Nicholson Street corridor* as shown in the *Brunswick Structure Plan Reference Document, Moreland City Council, 2018*, to invigorate street life and contribute to a safe and pedestrian friendly environment. The frontages are as follows:
 - Frontage Type A: Retail - A display window and/or entrance, measuring at least 80% of the width of the street frontage of each individual premises.
 - Frontage Type B: Commercial - A display window and/or entrance, measuring at least 60% of the width of the street frontage of each individual premise.
 - Frontage Type C: Residential - Individual entry doors to ground floor dwellings. Privacy is to be provided by elevating the ground floor approximately 0.5 to 1 metre above the street level and/or provide a landscaped front setback.
- Development should incorporate windows in all upper level facades facing streets.
- Development should minimise the impact of vehicle access and car parking on the public realm by:
 - Locating vehicle access at the rear of buildings where possible;

- On secondary frontages, rear access is preferred but where vehicle access is not possible, it should be provided to the side of corner properties away from the front facade; and
- Locating car parking where it cannot be seen from the public realm.
- In commercial and mixed use zones, development should have a ground floor ceiling height that allows for a range of appropriate uses.
- In commercial zones, development should incorporate awnings over the footpath for the full width of the building frontage.
- Services cabinets should be located at the rear of buildings wherever possible, or the side of corner buildings away from the front facade where rear access is not available. Where they can only be located in the front facade, the size of services cabinets should be minimised and they should be integrated with the overall facade design.
- Building facades on the street boundaries of Nicholson Street should incorporate vertical articulation to reinforce the prevailing pattern of subdivision and buildings.

Category 2 Employment Areas

- Development within Category 2 – Employment Areas should achieve commercial built form outcomes, including increased floor to ceiling heights and flexible floor plates, at ground and first floor to facilitate opportunities for employment uses.
- Ground floor commercial spaces should provide for activation, for example through entrances, transparent glazing and architectural features which encourage interaction and visual interest.

Exemption from notice and review

An application to construct a building or construct or carry out works is exempt from the notice requirements of Section 52(1) (a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the *Planning and Environment Act 1987* (the Act) unless the proposal seeks to do any of the following:

- Exceed the preferred maximum building height specified in section 2.0 of this schedule, or
- Exceed the preferred maximum street wall height specified in section 2.0 of this schedule, or
- Reduce the setbacks above the street wall specified in section 2.0 of this schedule, or
- Reduce the rear setback requirements specified in section 2.0 of this schedule.

3.0

14/05/2020
C164pt1moreAmore

Subdivision

Subdivision of existing sites not associated with a development proposal that achieves the design objectives and requirements of this schedule is discouraged.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0

14/05/2020 --/20--
C164pt1moreAmore
Proposed C212

Advertising Signs

None specified.

5.0

--/20--
Proposed
C212

Application Requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- For a development of four or more storeys:

- A computer model of the proposal compatible with Council's software;
- A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development;
- Streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape elevation;
- Three-dimensional perspective diagrams which show the form of the proposal at oblique views from neighbouring streets where any part of the proposal will be visible.

56.0

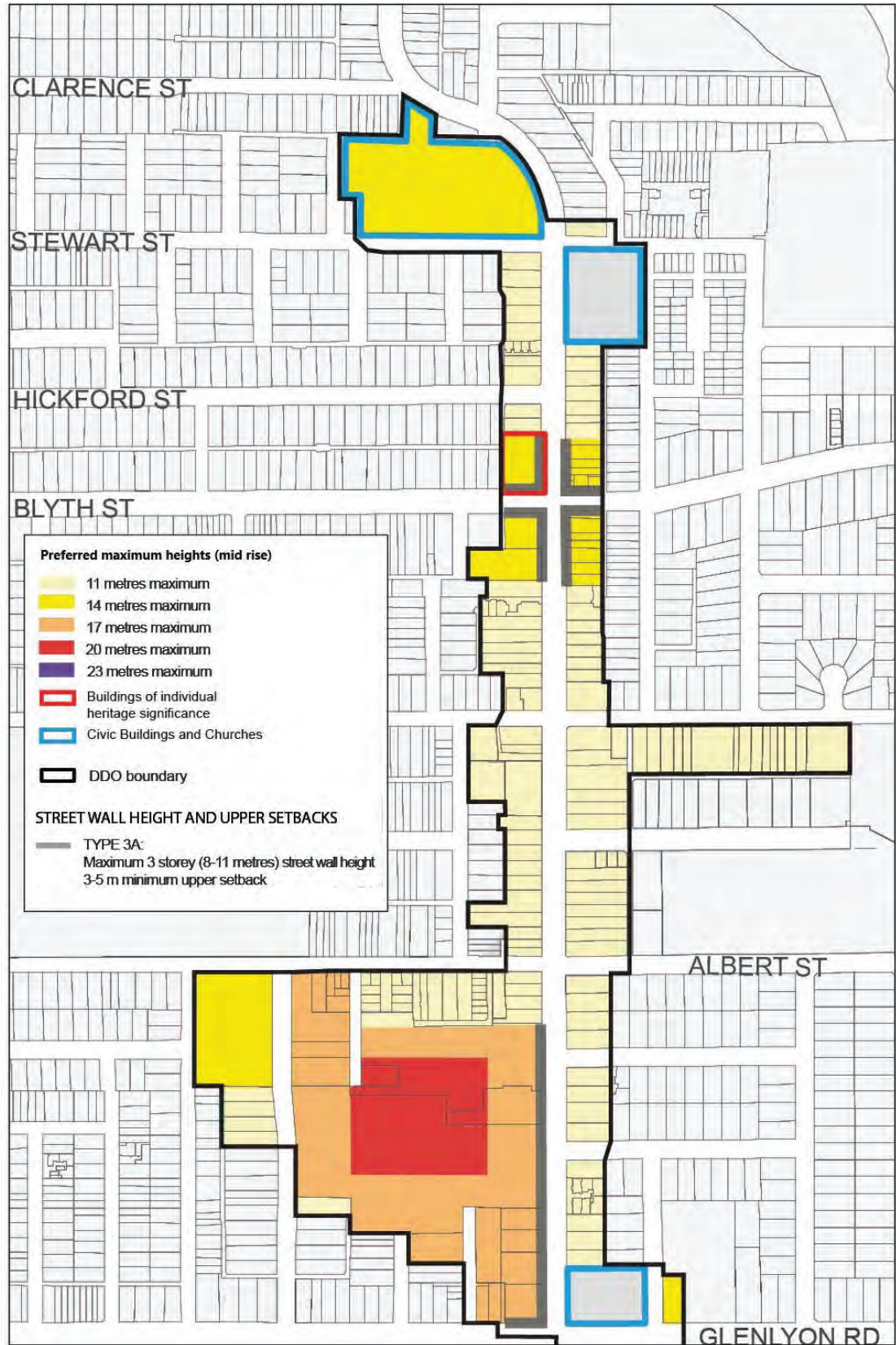
14/04/2021
 --/20--
 C167more
 Proposed
 C212

Decision Guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- The *Brunswick Structure Plan Reference Document, Moreland City Council, 2018*.
- How development within Category 2 Employment areas responds to the built form requirements for Category 2 Employment areas.
- Whether any proposal seeking to vary the upper level setbacks meet the design objectives of the schedule.
- The opportunities and constraints of the site, including whether the maximum building height is achievable having regard to lot size, dimensions (width and depth) and/or those sites within or adjoining the Heritage Overlay or any other relevant consideration.
- Whether an increased upper level setback is appropriate having regard to the heritage significance of the site or an adjoining site within the Heritage Overlay

Map 1A: Nicholson Street Local Area Built Form Controls (North)



Map 1B: Nicholson Street Local Area Built Form Controls (South)

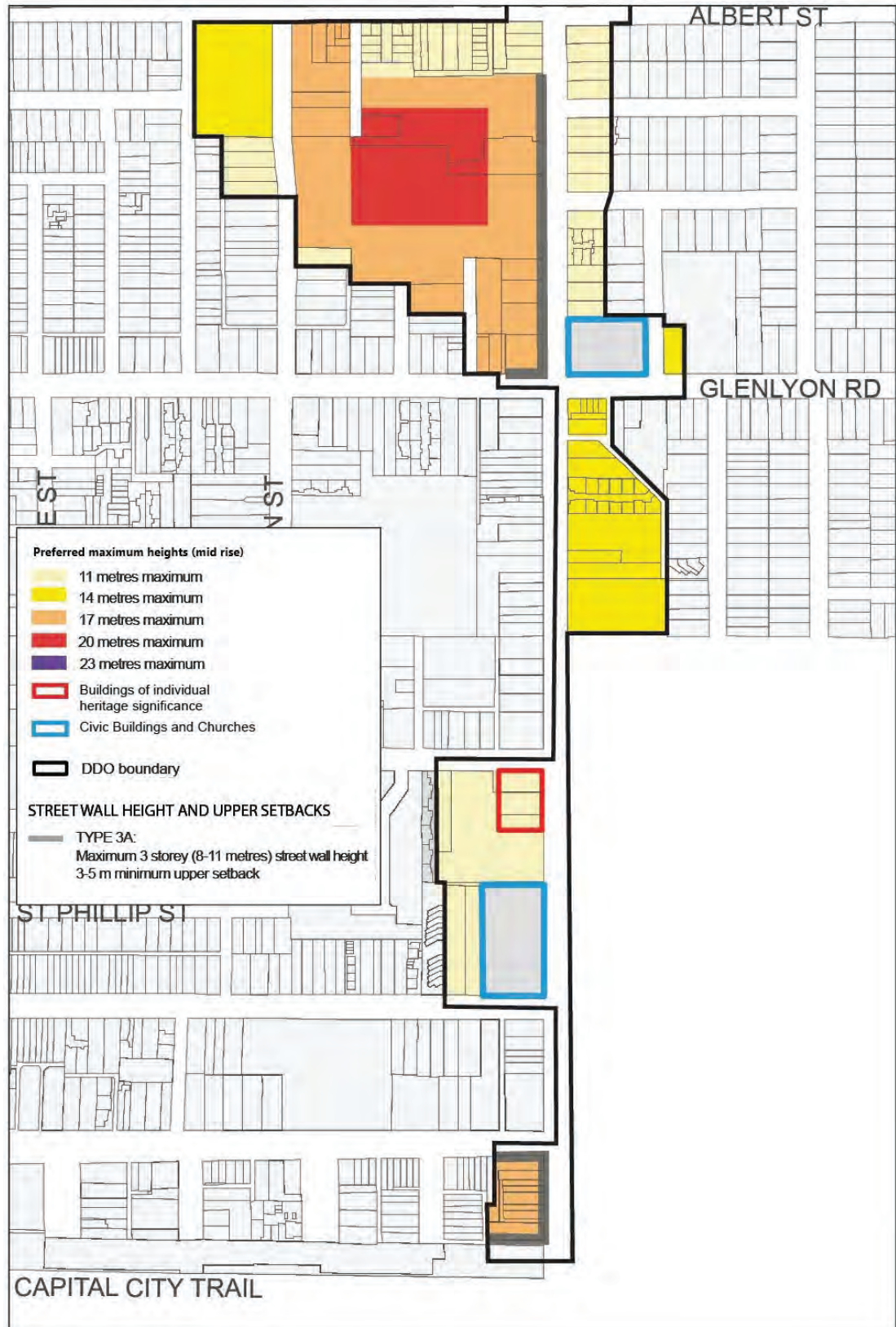
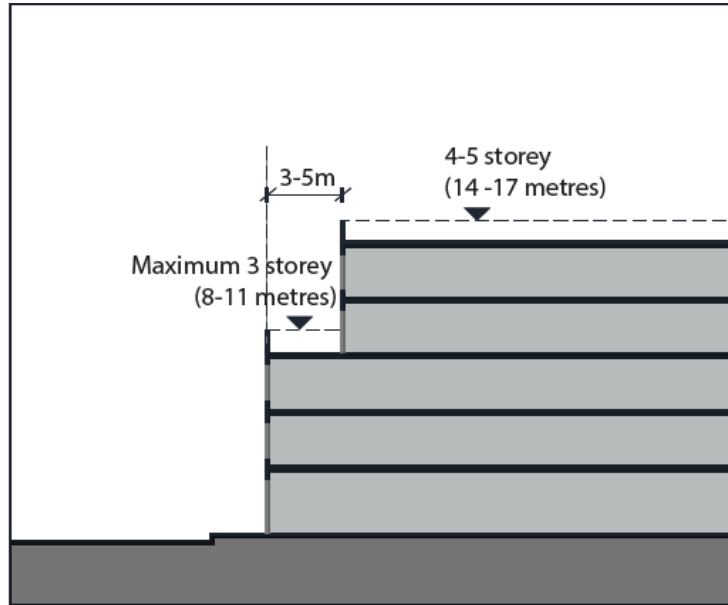


Diagram 1 Nicholson Street Local Area Section Diagram



TYPE 3A: Maximum 3 storey (8-11 metres) street wall height
3-5 m minimum upper setback

22/07/2010
C107

SCHEDULE 22 TO THE DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO22**

LAND BOUND BY BARKLY STREET TO THE NORTH, NICHOLSON STREET TO THE EAST, BRUNSWICK ROAD TO THE SOUTH, BRUNSWICK EAST

1.0

22/07/2010
C107

Design objectives

- To promote design excellence and high quality contemporary and innovative architecture throughout the precinct.
- To encourage development to provide high-density forms of residential accommodation above ground floor and first floor commercial spaces.
- To encourage the creation of a high quality public realm, active street frontages at ground level, linkages between Barkly Street and Brunswick Road and surveillance of streets.
- To encourage an equitable and reasonable standard of privacy, daylight, solar access, natural ventilation and outlook from within buildings in the precinct.
- To ensure that the height, siting and design of development does not adversely impact on the amenity of neighbouring properties.
- To avoid unreasonable visual bulk of development when viewed from surrounding streets and properties by ensuring appropriate scale, form and articulation.
- To establish a high quality architecturally designed signature building on the corner of Brunswick Road and Nicholson Street.
- To ensure that development within the precinct incorporates environmentally sustainable design.
- To ensure development provides noise attenuation measures where required that may be affected by established business operations and traffic.
- To ensure that development is designed to meet the access needs of all sectors of the community.
- To ensure that development does not result in safety and congestion issues at intersections with primary roads (Road Zone 1) or secondary roads (Road Zone 2).
- To satisfy the Roads Corporation's requirements to ensure that the development of land does not adversely affect the level of service, safety and amenity of the adjacent arterial road network.

2.0

22/07/2010 --/--/---
G407 Proposed C212more

Application Requirements ~~(4)~~

An application for development of land within the precinct must include, as appropriate, the following information to the satisfaction of the Responsible Authority:

- For a development of four or more storeys:
 - A computer model of the proposal compatible with Council's software;
 - A three-dimensional perspective which shows that proposed development within the streetscape in the context of adjacent development;
 - Streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape elevation;
 - Three-dimensional perspective diagrams which show the form of the proposal at oblique views from neighbouring streets where any part of the proposal will be visible.
- An Environmental Management Plan prepared by a suitably qualified environmental engineer that demonstrates how the development provides for environmentally sustainable design throughout its full life cycle to the satisfaction of the responsible authority.
The Environmental Management Plan should:

- Identify strategic or other documented sustainability targets or performance standards;
 - Document the means by which the appropriate targets or performance standards may be met;
 - Identify responsibilities and a schedule for implementation, and ongoing management, maintenance and monitoring where relevant; and
 - Demonstrate that the design element, technologies and operation practices included in the Environmental Management Plan can be maintained over time.
- An Acoustic Engineering Report prepared by a suitably qualified person demonstrating the use of suitable materials and building treatments to ensure internal noise levels are satisfactory.

3.0

22/07/2010
C107

Buildings and works

Built form

- Development of a signature building at the intersection of Nicholson Street and Brunswick Road should not exceed 25.2 metres (7 storeys).
- The height of new development at 39 – 47 Nicholson Street should not exceed 18 metres (5 storeys).
- Development fronting Barkly Street should not exceed 10.9 metres (3 storeys) in height unless otherwise specified in this schedule.
- The height of new development adjacent to 46 Barkly Street should not exceed 10.9 metres (3 storeys).
- Development fronting Brunswick Road should not exceed 14.5 metres (4 storeys) in height unless otherwise specified in this schedule.
- The height of new development adjacent 31 – 35 Brunswick Road should not exceed 10.9 metres (3 storeys).
- A maximum building height of 21.6 metres (6 storeys) will be considered for properties fronting Barkly Street and Brunswick Road, provided it is well setback from the street frontage and separated from residential interfaces.
- An application to vary the building heights must achieve exemplary architectural design and clearly demonstrate compliance with the design objectives of this schedule.
- The height requirements do not apply to any rooftop building, plant or equipment that is located and designed to minimise its visibility from any adjoining road to the satisfaction of the responsible authority.
- Demonstrate how the development will incorporate adaptable, accessible and visitable design features.

Pedestrian access, movement and amenity

Any new development should make a positive contribution to the pedestrian environment by:

- providing linkages between Brunswick Road and Barkly Street that caters for pedestrians and cyclists;
- providing high quality streetscape treatment (including street furniture, lighting and landscaping) along main pedestrian routes;
- providing a canopy for weather protection over the footpath to Brunswick Road and Nicholson Street where possible; and
- relocating affected and/or new utilities, cabling and service infrastructure underground where possible.

Car parking and vehicle access

- Car parking should be provided in a manner so as not to be intrusive to the design elements of structures and to not dominate the street frontage.
- Areas set aside for car parking are to be located to avoid fronting on to Brunswick Road, Nicholson Street or Barkly Street at either ground or podium levels.
- The number of access points to Brunswick Road, Nicholson Street and Barkly Street should be kept to a minimum.

Environmentally sustainable design

Development should incorporate, where appropriate, environmentally sustainable design measures in the areas of energy and water efficiency, passive solar design, natural ventilation, stormwater reduction and management, solar access, orientation and layout of the development, green travel and alternative transport modes, building materials and waste minimisation.

4.0

22/07/2010
C107

Exemption from notice and review

An application for buildings and works that is consistent with the *Brunswick Road, Nicholson Street, Barkly Street Development Precinct Height and Massing Study* 2009 prepared by Rothe Lowman for EG Funds Management is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

5.0

22/07/2010
C107

Decision guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- The design objectives and building and works requirements of this schedule.
- The architectural quality and innovative response of the building design.
- The extent to which the development makes a positive contribution to the overall vitality of the Brunswick Major Activity Centre and Nicholson Street/Brunswick Road Urban Village Precinct.
- The contribution the development makes to urban design, walkability, permeability and streetscape appearance of the area.
- The layout and appearance of areas set aside for vehicular access, loading and unloading and the location of any proposed car parking.
- The overall environmental sustainable performance of the development.
- The extent to which the development can accommodate an integrated mix of uses including office, retail and residential.
- Whether the development is sympathetic to adjoining uses and internal residential amenity is protected from adjoining non-residential uses.
- The extent to which the proposed development achieves the design objectives set out in the Design Guidelines for Higher Density Residential Development published by the Department of Sustainability and Environment (2004).
- Proposed access locations and treatments, and the impacts on traffic conditions on surrounding streets, as considered under a Transport Impact Assessment Report.
- The views of VicRoads and the Department of Transport.

6.0

22/07/2010
C107

References

Moreland Industrial Land Use Strategy, Hansen Partnership & National Economics, August 2004.

MORELAND PLANNING SCHEME

Design Guidelines for Higher Density Residential Development, Department of Sustainability and Environment, 2004.

Design Guidelines for Development of Four or More Storeys, Moreland City Council, June 2005.

Activity Centre Design Guidelines, Department of Sustainability and Environment, 2005.

Draft Brunswick Structure Plan 2008, Moreland City Council, 2008

Brunswick Road, Nicholson Street, Barkly Street Development Precinct Height and Massing Study, Rothe Lowman for EG Funds Management, June 2010.

03/08/2017
C163

SCHEDULE 23 TO THE DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO23**.

BALFE PARK PRECINCT, BRUNSWICK EAST

1.0

07/02/2013
C113

Design objectives

- To ensure that development is consistent with the objectives of the *Balfe Park Precinct, Brunswick East, Urban Design Framework (Figure 1), Brunswick Structure Plan* and Clause 22.11 *Development of four or more storeys*.
- To encourage multi-level development adjacent to Nicholson Street that provides transitional heights and design attenuation to Balfe Park and John Street.
- To support commercial development that provides opportunities for employment generating uses.
- To balance incremental multi-level development, internal amenity outcomes and development potential across the Precinct.
- To encourage development to provide high density forms of residential accommodation above ground floor commercial spaces.
- To integrate Balfe Park with the surrounding building form, streetscapes and movement network.
- To enhance the amenity and safety of Balfe Park and the public realm.
- To improve the permeability of the Precinct and the movement network for all modes, through the creation of links between Nicholson Street and Balfe Park and a multifunctional north south laneway between Glenlyon Road and Little Miller Street.
- To create a meeting place at the Nicholson Street end of the Balfe Park/Nicholson Street pedestrian link for passive recreation, informal meeting and waiting for trams.
- To ensure the scale of development creates a consistent Nicholson Street streetscape.
- To ensure building design incorporates Best Practice environmentally sustainable design initiatives.
- To ensure that development is designed to meet the access needs of all sectors of the community.
- To ensure new development provides noise attenuation measures.
- To encourage the reconfiguration and consolidation of land within the Precinct.
- To satisfy the Roads Corporation and tram authority requirements to ensure that development does not adversely affect the level of service, safety and amenity of the adjacent arterial road and tram network.

2.0

07/02/2013 --/--/----
G443 Proposed C212more

Buildings and works ~~(4)~~

Application requirements

An application for development must include, as appropriate, the following information to the satisfaction of the responsible authority:

- For a development of four or more storeys:
 - a computer model of the proposal compatible with Council's software;
 - a three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development;

- streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape elevation;
 - three-dimensional perspective diagrams which show the form of the proposal at oblique views from neighbouring streets where any part of the proposal will be visible.
- An Environmentally Sustainable Design Management Plan prepared by a suitably qualified person(s), demonstrating Best Practice in environmentally sustainable design. The Environmental Management Plan should:
 - Identify strategic or other documented sustainability targets or performance standards;
 - Document the means by which the appropriate targets or performance standards may be met;
 - Identify responsibilities and a schedule for implementation, and ongoing management, maintenance and monitoring when relevant; and
 - Demonstrate that the design elements, technologies and operation practices included in the Environmental Management Plan can be maintained over time.
 - An Acoustic Engineering Report prepared by a suitably qualified person demonstrating the use of suitable materials and building treatments to ensure internal noise levels are satisfactory.
 - A Traffic Impact and Management Report prepared by a suitably qualified person that identifies:
 - onsite car parking provisions;
 - expected traffic volumes and impact on the existing road network, including impacts on the operational efficiency and road safety at the Nicholson Street/ Miller Street intersection;
 - necessary intersection treatments and traffic management devices;
 - upgrades or modifications to existing roads;
 - provision of new roads;
 - internal street functional hierarchy;
 - impact on pedestrian and bicycle routes, and
 - measures to reduce conflict and improve pedestrian and bicycle amenity.
 - A Green Travel Plan prepared by a suitably qualified person outlining site-specific initiatives and actions to encourage the use of more sustainable transport options.
 - An Accessibility Report prepared by a suitably qualified person detailing how the development will incorporate adaptable, accessible and visitable design features, including accordance with *Australian Standard 4299-1995: Adaptable Housing*.
 - A detailed response as to how the development achieves the objectives of Clause 22.11 *Development of four or more storeys*, as appropriate.
 - An Urban Context and Design Response Report for development which seeks to vary the preferred building heights and podium setbacks in Figures 1 and 2 that demonstrates how the design objectives and requirements of this schedule are met.

Built Form

- The height and form of development, including active and activated edges, should be in accordance with Figures 1 and 2.
- Development should be designed to provide naturally lit internal amenity.
- Development should consider height and shadowing impacts to adjoining properties, in particular access to sunlight and the development objectives of this schedule.

- Development that interfaces with Balfe Park should avoid unreasonable visual bulk when viewed from the park.
- Properties fronting Nicholson Street should have a zero lot line, and clear glazed frontages with canopies at ground level that contribute to a consistent preferred streetscape.
- Residential entries fronting Nicholson Street should be designed to limit indentation and not detract from the commercial function of the street.
- Development must incorporate a minimum three (3) metre setback at ground and upper levels, as shown in Figure 1, to:
 - provide for a continuous laneway connection between Glenlyon Road and Little Miller Street.
 - improve rear vehicle access to properties fronting Nicholson Street.
- Development should be oriented to Balfe Park and create activated edges at ground level and passive surveillance at upper levels, particularly along the north-south laneway link, that include but are not limited to, building frontages, prominent pedestrian entrances, links to Nicholson Street, private open space and windows.
- Development should front and activate the new pedestrian connection/s between Nicholson Street and Balfe Park where possible, to create welcoming, safe and functional pedestrian thoroughfares.
- The meeting place is to be created by setbacks to the ground floor corners of the buildings adjacent to the Nicholson Street entrance of the east-west pedestrian link, and designed to provide a high amenity space through the creation of seats, shelter, landscaping and access to sunlight.
- Development fronting Miller Street and Little Miller Street should provide front setbacks for gardens and landscaping.
- Developments should be designed to be adaptable in accordance with *Australian Standard 4299-1995: Adaptable Housing*.

Pedestrian access, movement and amenity

- The design and siting of development should provide for enhanced and new pedestrian and cyclist links through the Precinct, and a meeting place, particularly between Nicholson Street and Balfe Park, as detailed in Figure 1.

Car parking and vehicle access

- Development must incorporate rear setbacks to accommodate the provision and construction of a continuous 3 metre wide laneway, in addition to vehicle access requirements that connects Glenlyon Road to Miller Street via Little Miller Street, and utilises appropriate traffic calming measures.
- Laneway design and function must not be vehicle traffic dominated, but treated as a compatible shared pedestrian, bicycle and vehicle space.
- Vehicular access should be from rear lanes, or where the *Balfe Park Precinct, Brunswick East, Urban Design Framework* (Figure 1) outlines access via a new lane. Where no lane is available, access should be from side streets.
- Vehicle access points to Nicholson Street should be made redundant and kerb reinstated.
- Vehicle ingress and egress into development, including loading facilities and building servicing, must be designed to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.
- Car parking facilities should not dominate the public realm and be integrated into the architecture of the building. Basement car parks should be naturally ventilated.

Environmentally Sustainable Design

- Development must incorporate, where appropriate, Best Practice environmentally sustainable design and address the following:
 - Building energy management (conservation and generation);
 - Water sensitive urban design/integrated water management;
 - Construction materials selection;
 - Indoor environment quality;
 - Waste management and reduction;
 - Transport;
 - Demolition and construction phase;
 - Innovation;
 - Public realm design; and
 - Urban ecology.

Noise attenuation

- Any new development that will accommodate residential or other noise sensitive uses must be sited and designed to incorporate noise attenuation measures to minimise tram noise levels and/or other external noise sources within the development or adjoining uses to the satisfaction of the responsible authority.
- Development that incorporates sensitive uses must have regard to existing commercial and industrial use and incorporate a high level of noise attenuations so as to limit the impact of the sensitive use on existing businesses in the Precinct.
- Noise attenuation measures must be integrated with urban design objectives to achieve articulated facades and active frontages.

3.0

07/02/2013
C113

Subdivision

- Consolidation of land to facilitate the creation of viable development sites is encouraged.

4.0

07/02/2013
C113

Decision guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- The *Brunswick Structure Plan and Balfe Park Precinct, Brunswick East, Urban Design Framework*.
- Whether the proposed development achieves the design objectives and requirements of this schedule.
- The extent to which the development makes a positive contribution to the overall social, economic and environmental vitality of the Nicholson Street Local Area 3B and broader Brunswick Major Activity Centre.
- Whether development contributes to the function and amenity of the north-south laneway between Glenlyon Road and Miller Street.
- Whether development provides an appropriate transition in height and design detail to Balfe Park and surrounding properties.
- The contribution the development makes to urban design, walkability, permeability and streetscape appearance of the area.

- Whether the development improves the amenity and connections with Balfe Park.
- Whether the proposal incorporates public benefits such as affordable housing, community space, landscaping, or other physical improvements or services for the local community.
- The extent to which the development can accommodate an integrated mix of uses including large format retail, showroom or offices.
- Whether the development allows for the future development of adjacent land in a form consistent with the design objectives of this schedule and the Brunswick Structure Plan.
- The views of VicRoads and the Department of Transport.

5.0

03/08/2017
C163

References

Brunswick Structure Plan, Moreland City Council, August 2010

Balfe Park Precinct, Brunswick East, Urban Design Framework, Hansen Partnership Pty Ltd, October 2011

Figure 1 – Balfe Park Precinct, Brunswick East, Urban Design Framework (including preferred heights)

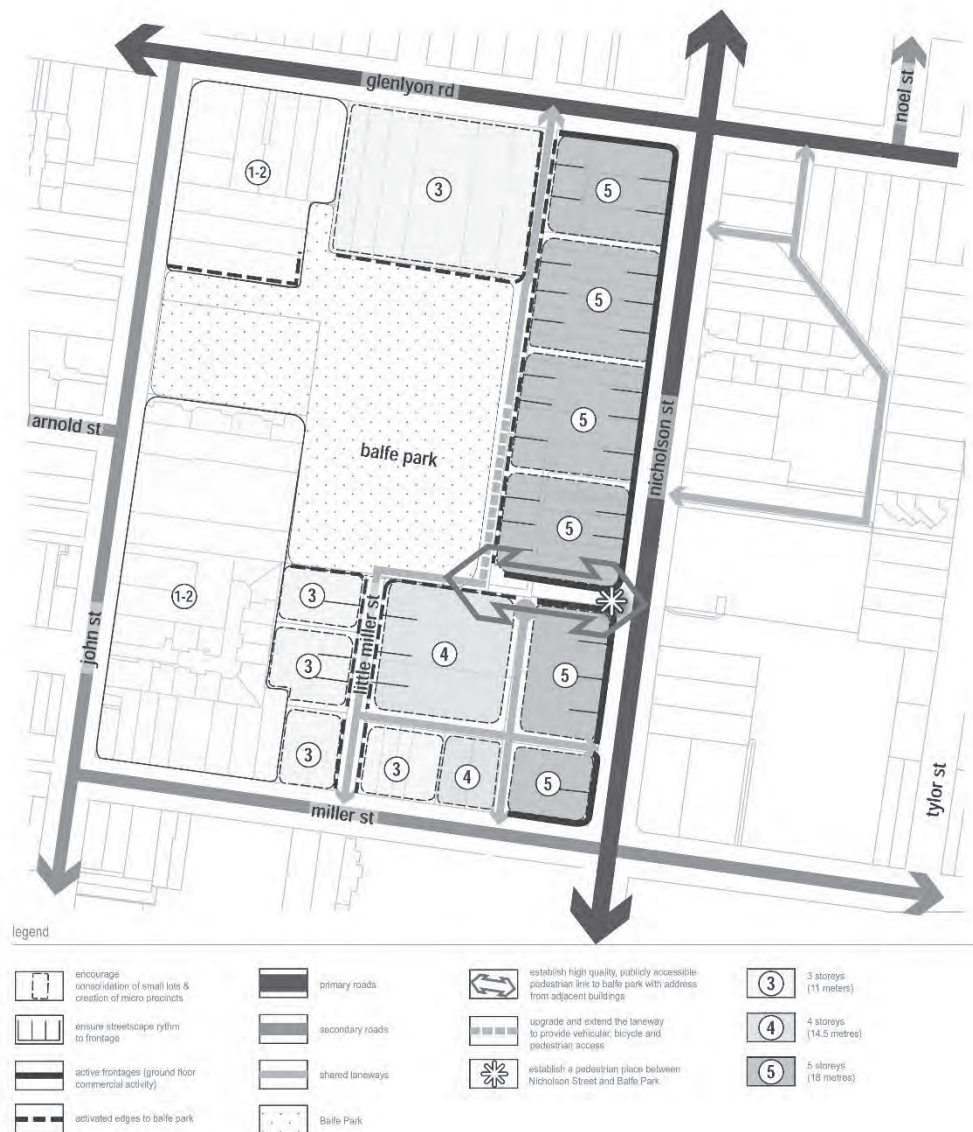
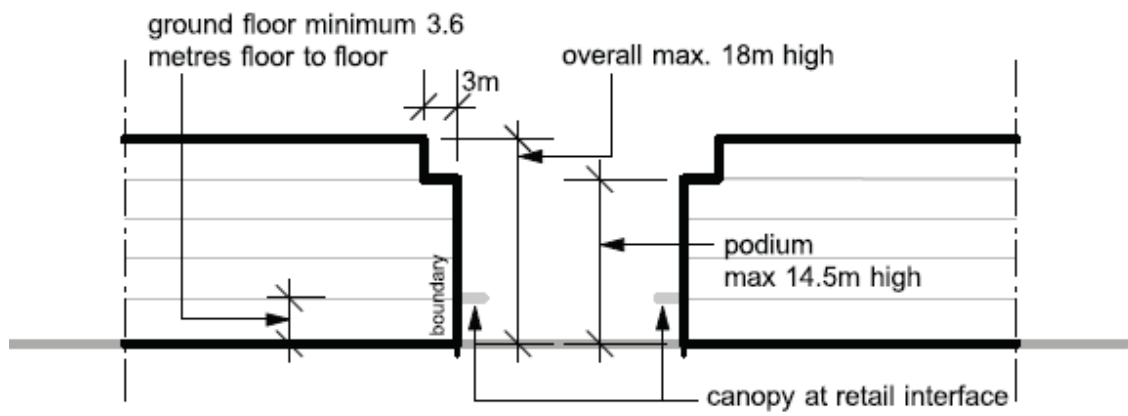


Figure 2 – Built Form Envelope



Source: *Brunswick Structure Plan*, Moreland City Council, August 2010

14/12/2017
C159

SCHEDULE 24 TO THE DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO**

NEIGHBOURHOOD CENTRES

1.0

14/12/2017
C159

Design objectives

- To implement the Moreland Neighbourhood Centres Strategy, 2017.
- To ensure built form outcomes are appropriate to the context of Moreland's Neighbourhood Centres.
- To improve the quality of higher density and mixed use developments by providing appropriate built form guidance.
- To improve amenity outcomes for residents in higher density and mixed use developments and for residents in adjacent buildings.
- To ensure that new development makes a positive contribution to the public realm.

2.0

14/12/2017
C159

Buildings and works

A permit is not required to:

- Install an automatic teller machine.
- Alter an existing building facade provided that:
 - The alteration does not include the installation of an external roller shutter; and
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing for development in the Commercial 1 zone.

Building Height Requirements

Any new building must not exceed the maximum height shown on the maps in Subclause 7.0 to this schedule. The maximum heights shown for the following neighbourhood centres cannot be varied with a permit:

Figure 7: Bell Street/Melville Road, Pascoe Vale South

Figure 8: Gaffney Street/Pascoe Vale Station

Figure 9: Gaffney Street/Sussex Street, Coburg North

Figure 10: West Street, Hadfield

Figure 11: Bonwick Street, Fawkner

Figure 12: Snell Grove, Oak Park

Figure 13: Merlynston Station, Merlynston

Figure 14: Moreland Road/Melville Road, Brunswick West.

The maximum building height requirement may be exceeded for architectural features and service equipment including plant rooms, lift overruns, structures associated with green roof areas and other such equipment if all of the following are met:

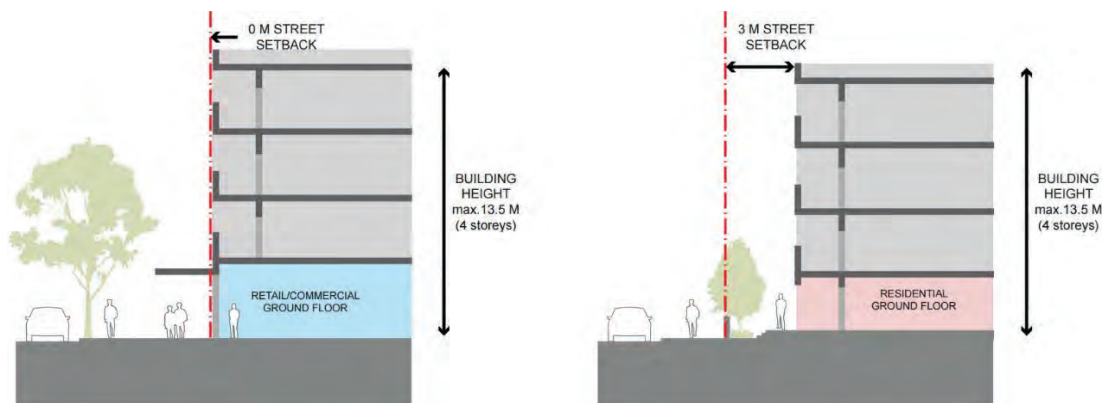
- No more than 50% of the roof area is occupied by the equipment (other than solar panels).
- The equipment is located in a position on the roof so as to minimise additional overshadowing on neighbouring properties and public spaces.
- The equipment does not extend higher than 1.2 metres above the maximum building height.
- The equipment and screening is integrated into the design of the building to the satisfaction of the responsible authority.

Front Setback Requirements

- All buildings with retail and commercial uses at ground floor should be constructed to the street boundary as shown in Figure 1. This may be varied, if the setback is:
 - Not required to create a consistent street wall.
 - Designed as part of the public realm.
 - Fully accessible to the public, or offices are proposed at ground floor.
- All buildings with residential uses at ground floor should be setback in accordance with Clause 55.03-1 Standard A3 or B6, or 3 metres, whichever is lesser, as shown in Figure 1.
- On corner sites the setback from the main street frontage should wrap around the corner then graduate to match the setback of the secondary street, or be set back 3 metres, whichever is the lesser.

Ground floor awnings, architectural features, sunshades and artworks may encroach into the front setback. No other part of a building including balconies may encroach into the front setback.

Figure 1: Front setbacks for retail/commercial ground floor and residential ground floor



Side Setback Requirements

With No Outlook

- Within the Commercial 1 zone: 0 metres from the property boundary (build to side boundaries to create continuous street edge).
- Any part of a building abutting a property in any other zone should comply with the side setback requirements of Clause 55 (ResCode Standard A10 or B17).

With Outlook

- Primary outlook (living/balcony outlook): 4.5 metres from the property boundary should be planted with trees.
- Secondary outlook (bedrooms outlook): 2 metres from the property boundary should be planted with trees.
- Any part of a building abutting any other zone must comply with the side setback requirements of Clause 55 (ResCode Standard A10 or B17).

Separation of buildings within sites should have regard to the building separation distances in the *Moreland Apartment Design Code, September 2017*.

Architectural features, sunshades and artworks may encroach into the side and rear setbacks. No other part of a building, including balconies, may encroach into the setback.

Rear Setback Requirements

- Buildings abutting a property boundary should be setback:
 - 3 metres from the property boundary for any part of a building up to a height of 4 metres as shown in Figure 2.
 - A minimum of 6 metres from the property boundary for any part of a building exceeding 4 metres, as shown in Figure 2.
- A minimum of 8.6 metres for any part of a building exceeding 10.5 metres adjacent to a site in the Neighbourhood Residential zone, as shown in Figure 3.

Laneways to the rear of a property in the Commercial 1 zone (with the exception of properties adjacent to the Neighbourhood Residential zone) should be counted as part of the setback, as shown in Figure 2.

Architectural features, sunshades and artworks may encroach into the rear setbacks. No other part of a building, including balconies, may encroach into the setback.

Figure 2: Rear Building Setbacks

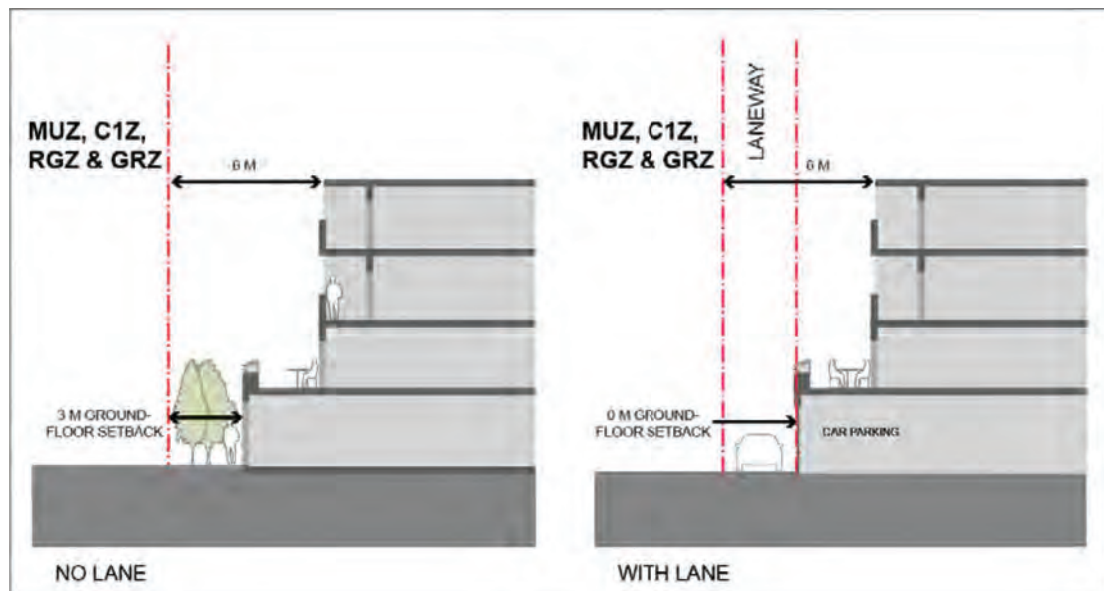
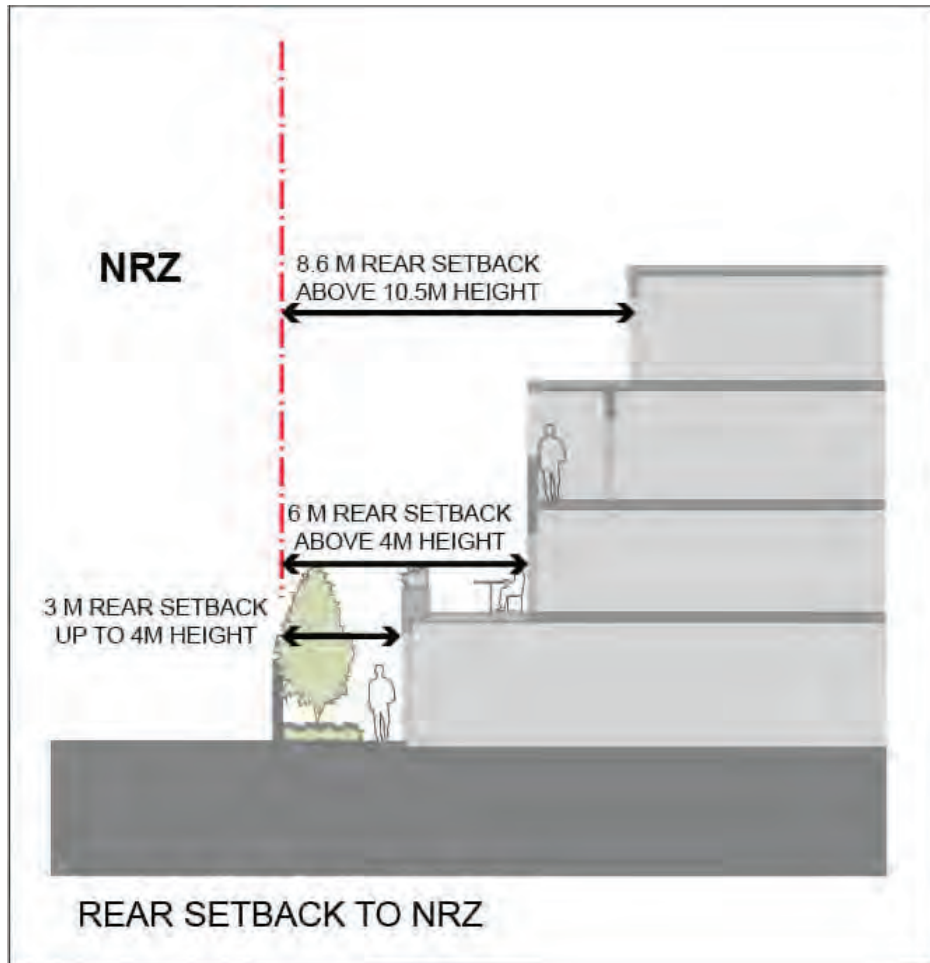


Figure 3: Rear building setbacks for sites adjacent to the Neighbourhood Residential one



Building Frontage Requirements

Active Frontages

- Development should incorporate the following at street level fronting all streets (except rear laneways) and public open spaces:
 - Commercial – A display window and/or entrance, measuring 65 - 80% of the width of the street frontage of each individual premise. Large signage and/or non-transparent glazing should be avoided. Non-transparent glazing up to a height of 1.7 metres may be used for office uses.
 - Residential – Individual entries to ground floor dwellings and habitable room windows facing the street. Privacy should be provided by elevating the ground floor approximately 0.5 to 1 metre above the street level and/or providing landscaping in a front setback.
- Parks and public spaces should be substantially fronted by dwellings and/or other occupancies with direct access and outlook. Separation between public and private land should be clear and may be delineated by paths and/or streets.
- On corner lots active frontages should address both streets. Blank side walls and associated garages/car parking are discouraged.

Awnings

- Development with retail and commercial uses at ground floor should incorporate awnings designed to provide adequate weather protection over the footpath for the full width of the building frontage.
- New awnings should match the existing height and depth of existing awnings

Building Articulation

- Building façades should be designed with an appropriate rhythm and proportion that respond to the building's uses and context, and contribute to a fine grain urban character.
- New buildings should adopt solid architectural expression that emphasises the street edge through the use of recessed balconies, framed elements and solid balustrades.
- Side setbacks should incorporate articulation to break down building mass through the use of materials and finishes.

Car Parking and Vehicle Entry

- The impact of vehicle access and car parking on the public realm should be minimised by:
 - Locating vehicle access to the rear of buildings wherever possible, or to the side, where rear access is not available.
 - Locating car parking where it cannot be seen from the public realm.
- Locate vehicle crossings to minimise the removal of street trees, on-street car parking spaces, or encroachment into landscaped front setbacks.
- Pedestrian safety and sight lines should be maximised.
- Where car stackers are proposed, ensure they are suitably located to minimise adverse amenity impacts to adjoining sites.

Site Services

- Service infrastructure should be concealed or integrated into building design.
- Waste storage areas should be screened from view of the street.

Landscaping

- Provide at least one tree in the front setback, selected in accordance with the *Moreland Tree Planting Manual, 2011*.
- Provide tree planting along the side and rear boundaries for properties located in the Residential Growth zone and Mixed use zone.

3.0

14/12/2017
C159

Subdivision

None specified

4.0

14/12/2017
-/-/20-
C159
Proposed
C212

Advertising Signs

None specified

5.0

-/-/20
Propose
d C212

Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- For a development of four or more storeys:
 - A computer model of the proposal compatible with Council's software;

- A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development;
- Streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape elevation;
- Three-dimensional perspective diagrams which show the form of the proposal at oblique views from neighbouring streets where any part of the proposal will be visible.

56.0

~~14/12/2017~~
~~-/-/20~~
~~C459~~
Proposed
C212

Decision guidelines

Before deciding on an application, the responsible authority must consider, as appropriate:

- Whether the development is consistent with the *Moreland Neighbourhood Centres Strategy 2017* and any other policy or planning provision applying to the area.
- Whether an appropriate Building Type has been proposed as per Appendix One of the *Moreland Neighbourhood Centres Strategy, 2017*.
- Whether the development provides an appropriate transition to properties in an adjoining zone.
- The extent to which the development achieves the design objectives in clause 1.0 and requirements of clause 2.0 of this schedule.

67.0

~~14/12/2017~~
~~-/-/20~~
~~C459~~
Proposed
C212

References

Moreland Neighbourhood Centres Strategy, 2017

Neighbourhood Centre Framework Plans

Figure 4: Grantham Street/Union Street Brunswick West Neighbourhood Centre

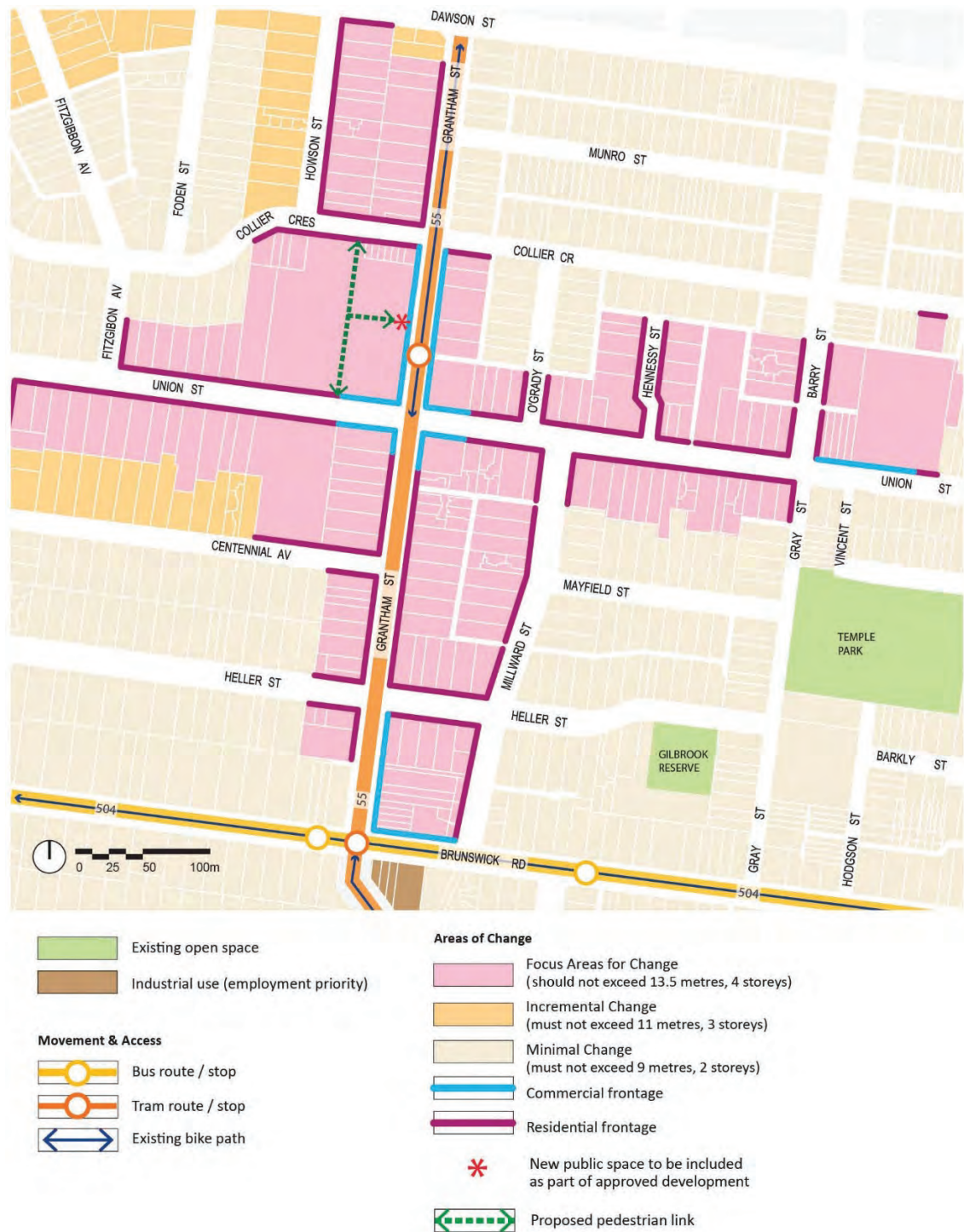
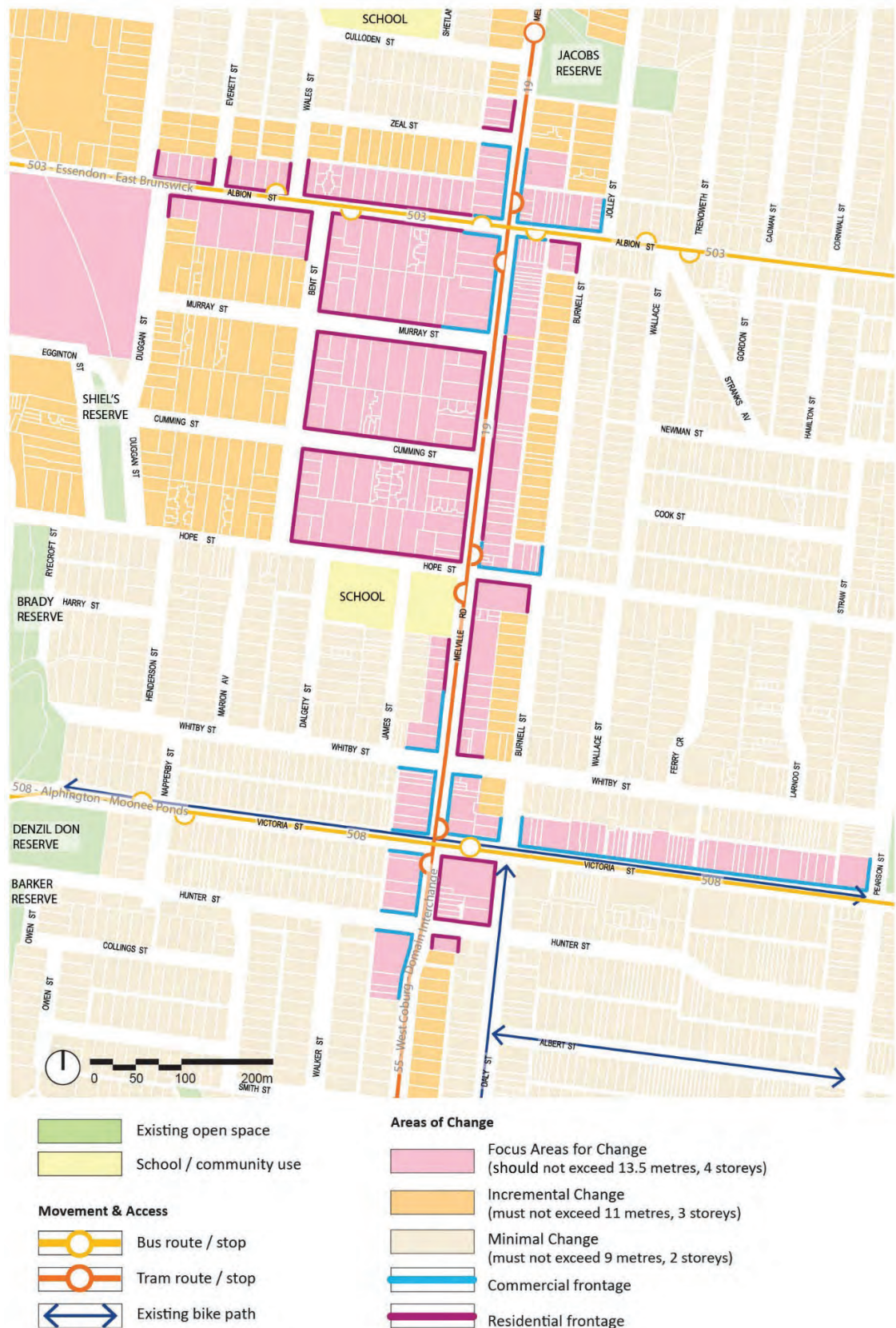


Figure 5: Melville Road/Albion Street/Victoria Street Brunswick West Neighbourhood Centre



MORELAND PLANNING SCHEME

Figure 6: Nicholson Street/Holmes Street/Moreland Road Coburg Neighbourhood Centre

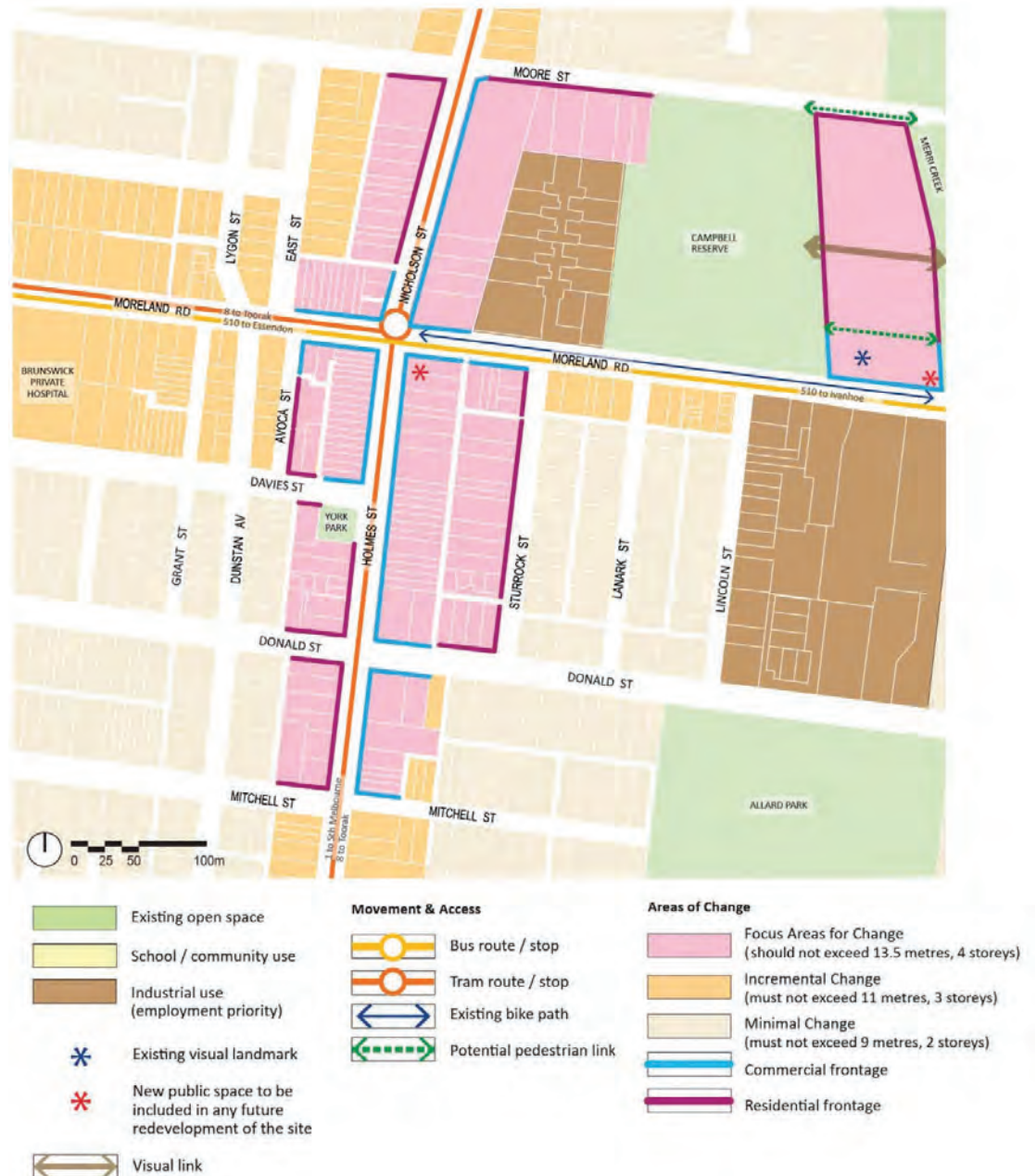


Figure 7: Bell Street/Melville Road Pascoe Vale South Neighbourhood Centre

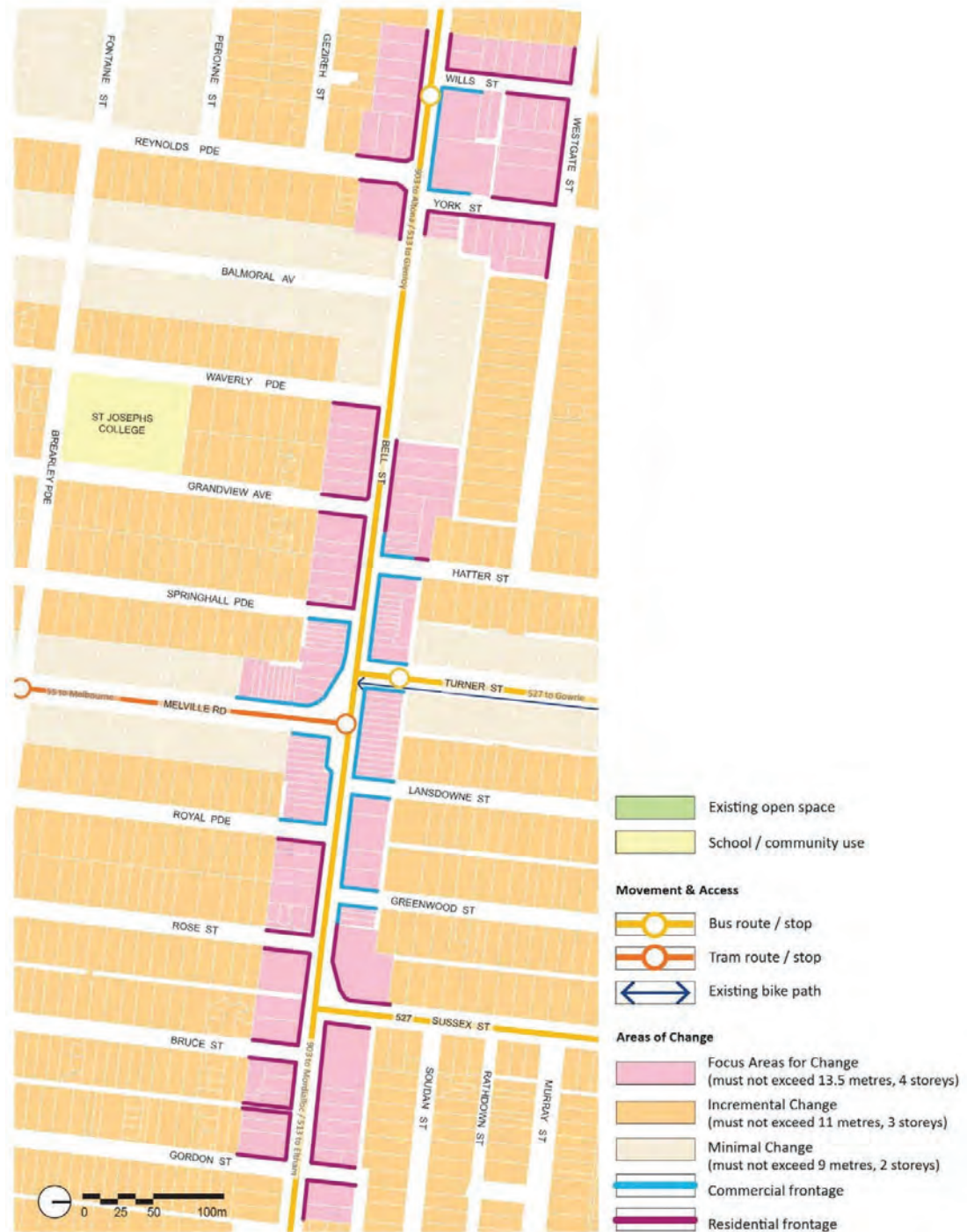


Figure : Gaffney Street/Pascoe Vale Station Pascoe Vale Neighbourhood Centre

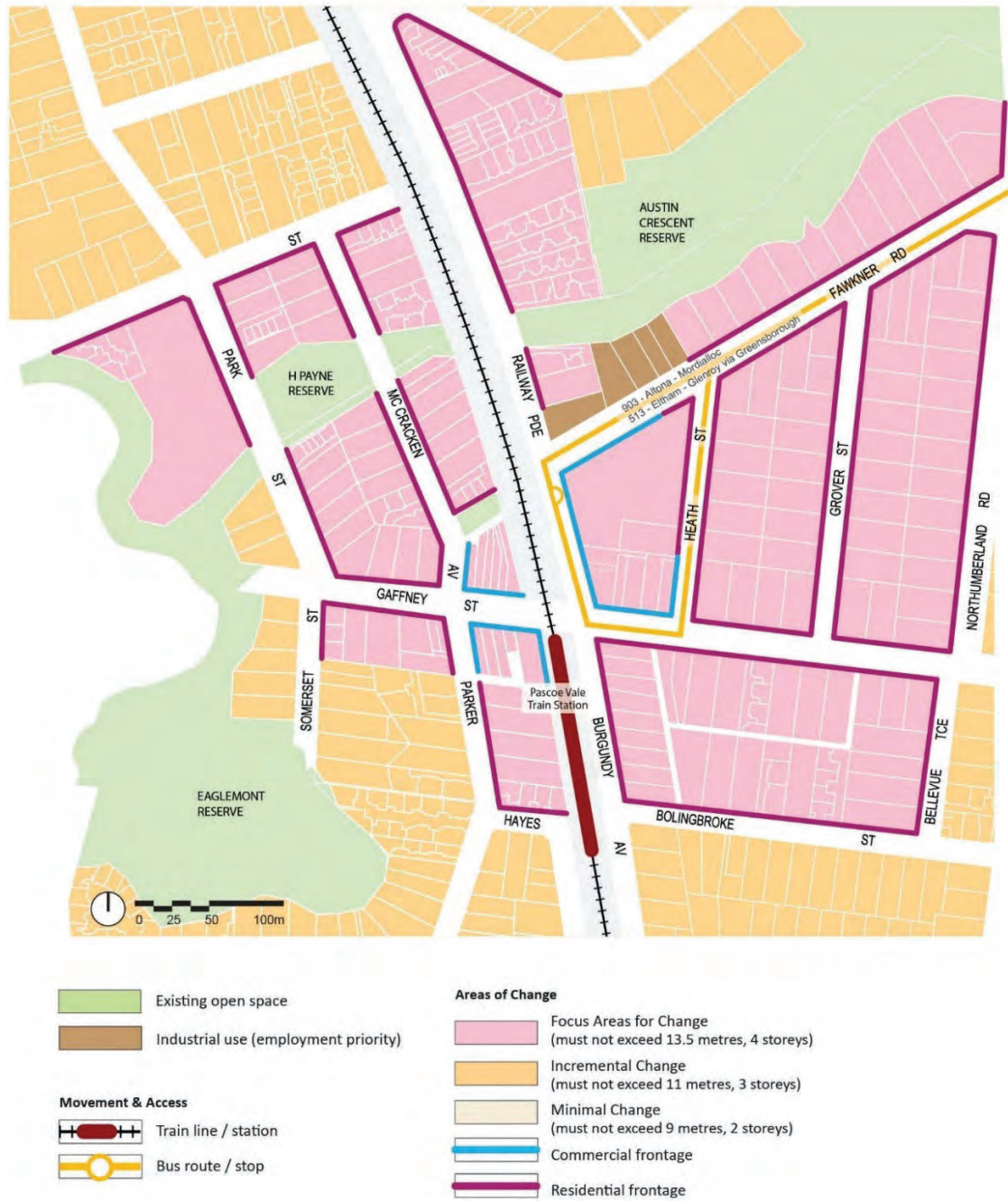


Figure 9: Gaffney Street/Sussex Street Coburg North Neighbourhood Centre

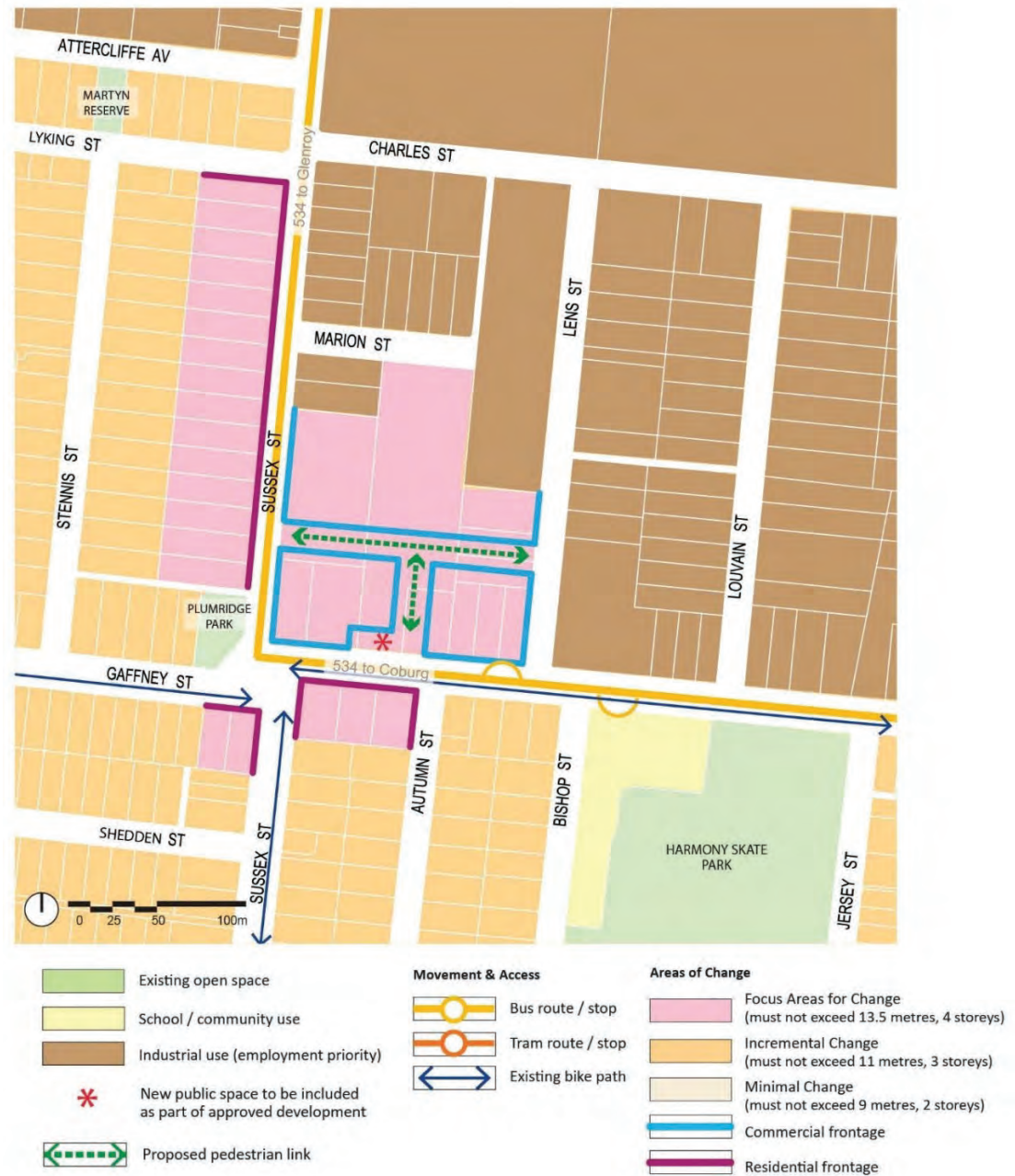


Figure 10: West Street Hadfield Neighbourhood Centre

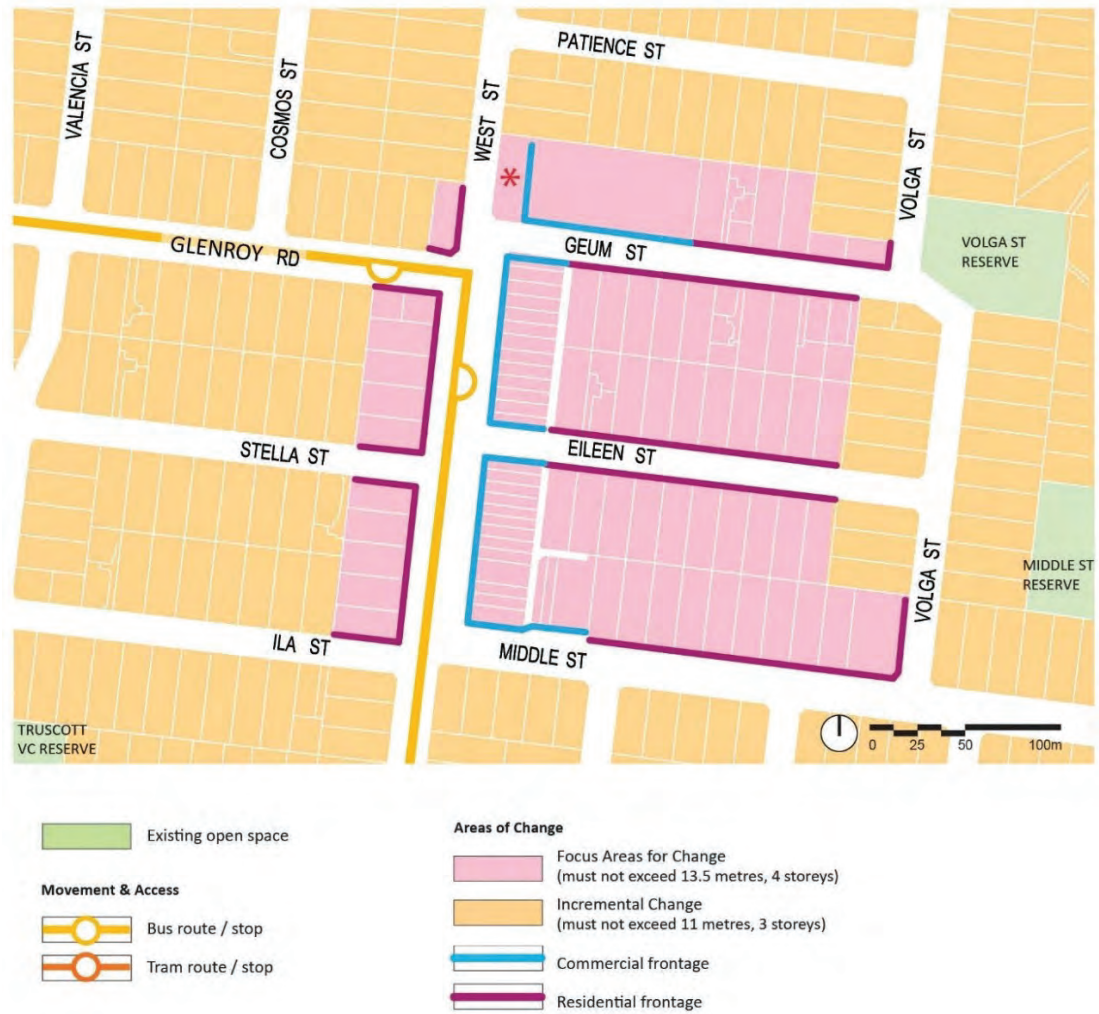


Figure 11: Bonwick Street Fawkner Neighbourhood Centre

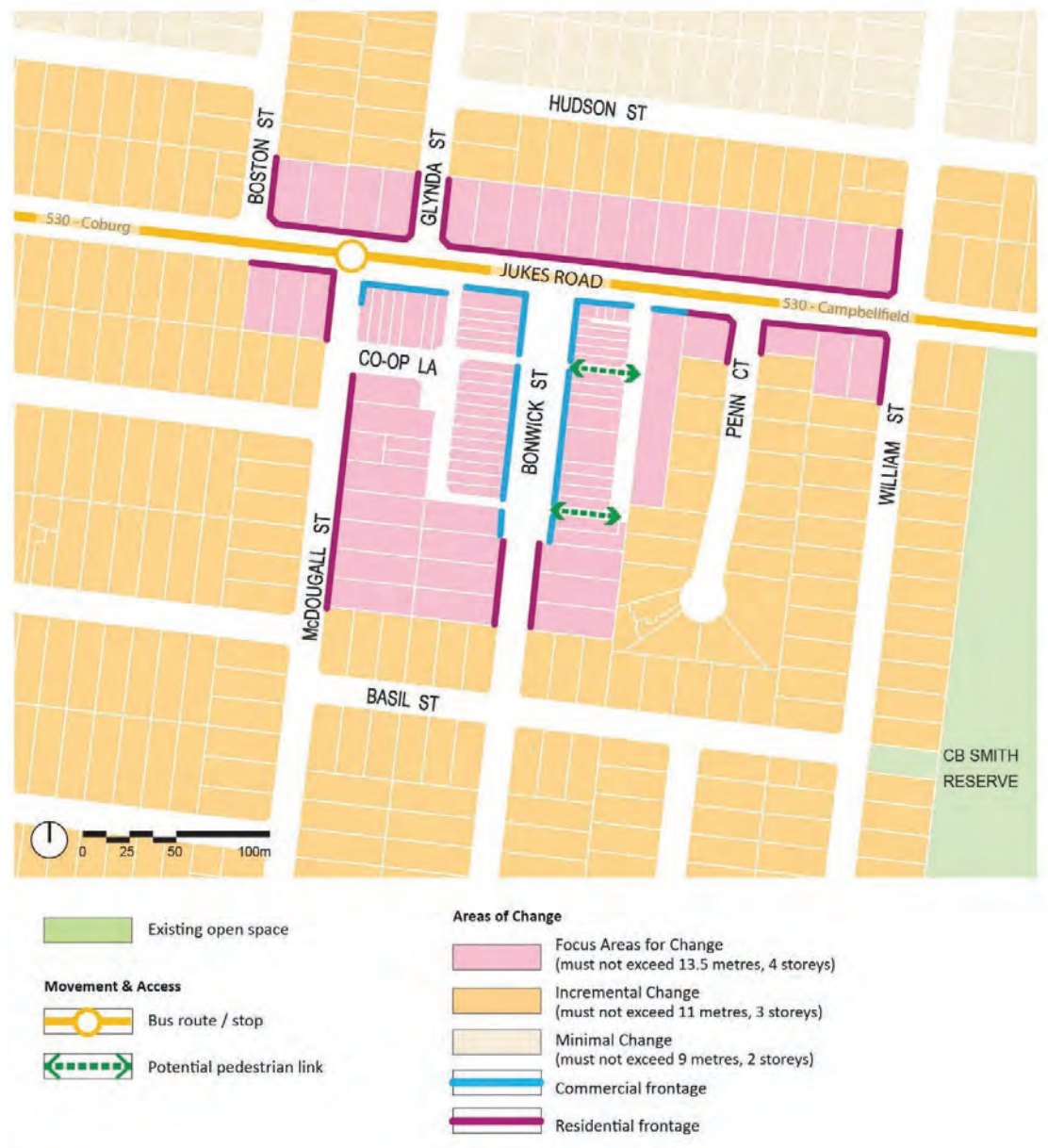


Figure 12: Snell Grove Oak Park Neighbourhood Centre

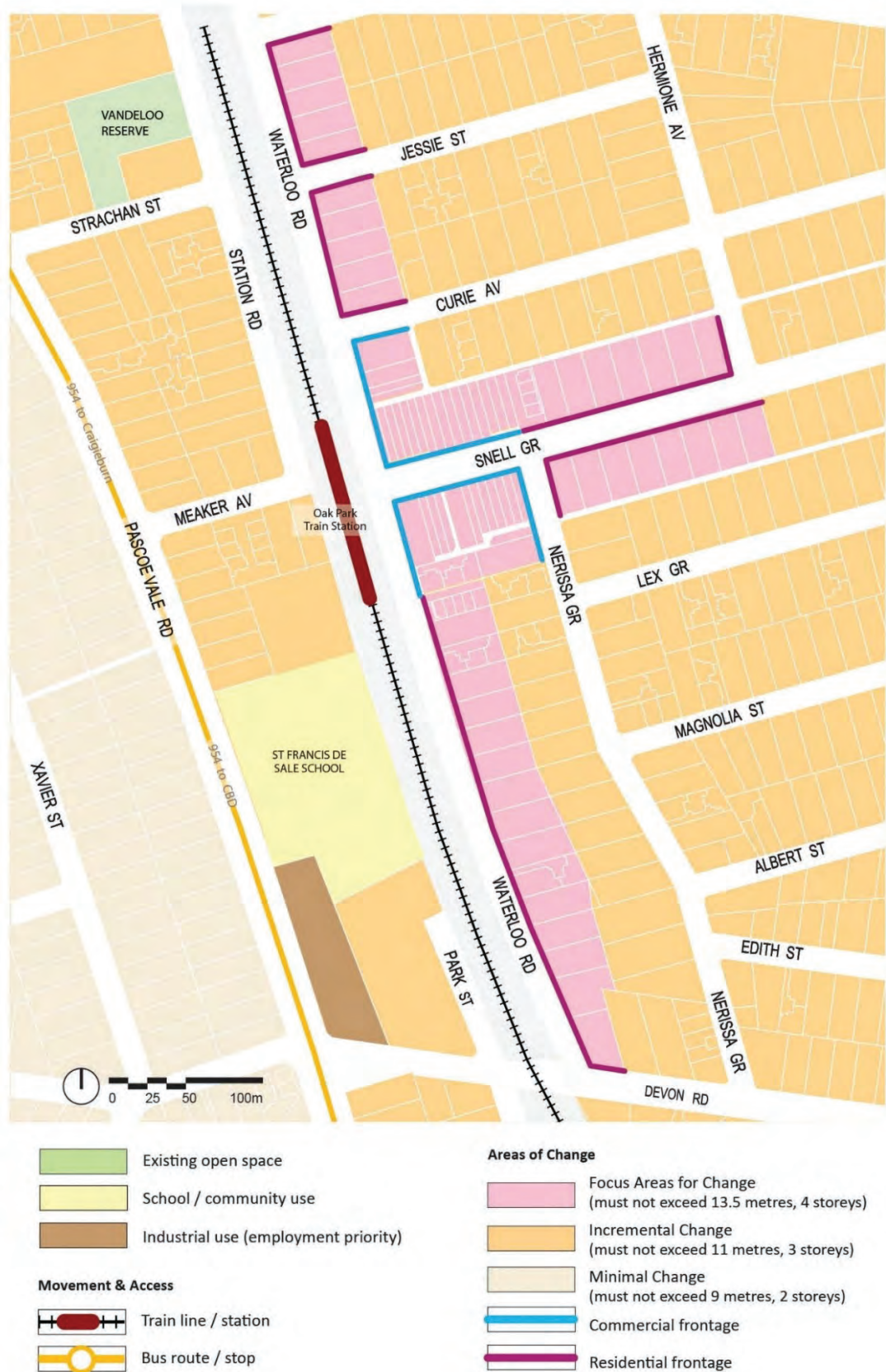


Figure 13: Merlynston Station Merlynston Neighbourhood Centre

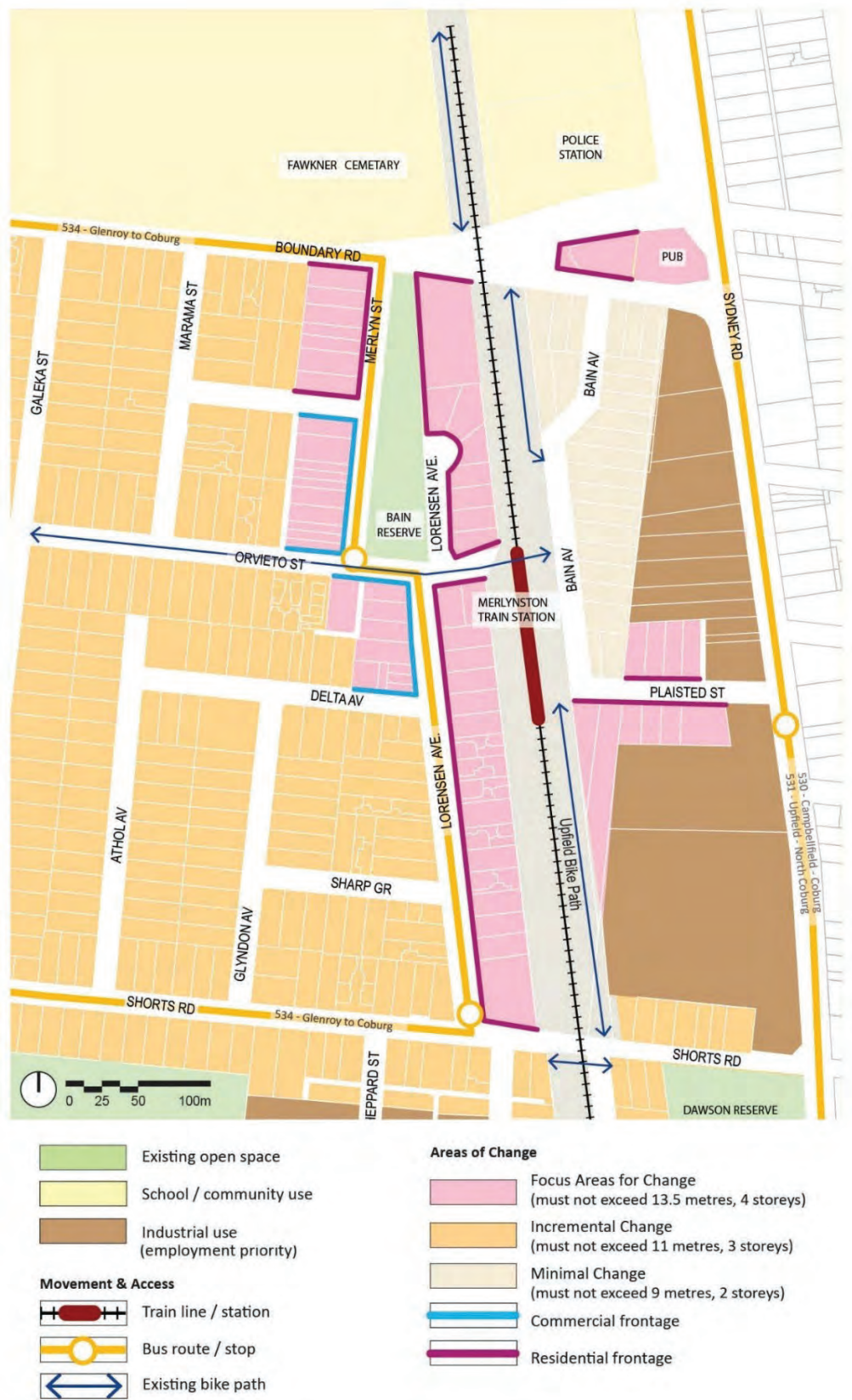
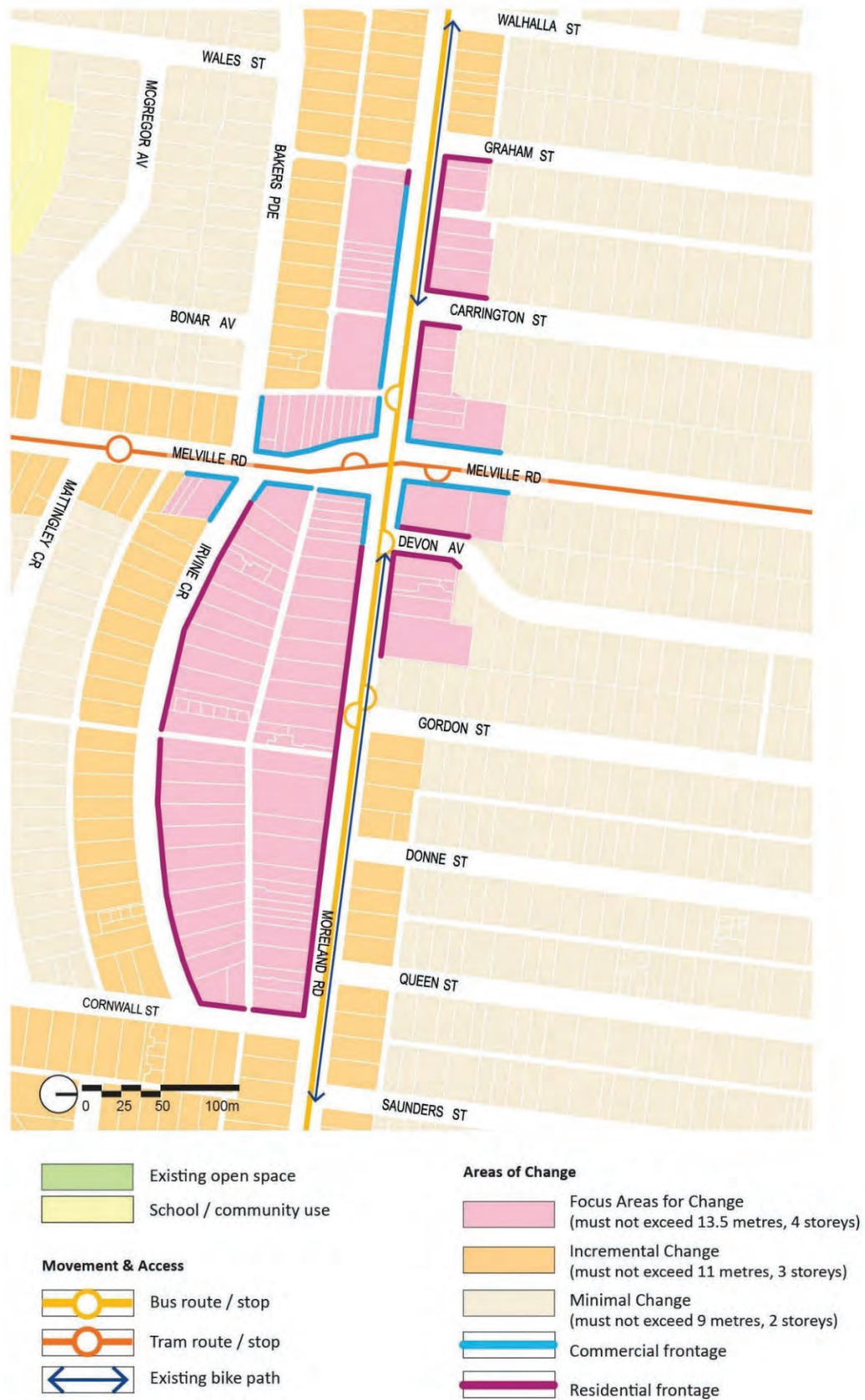


Figure 14: Moreland Road/Melville Road Brunswick West Neighbourhood Centre



02.03-2 Environmental and landscape values

--/--
C212

~~Moreland is a highly urbanised municipality with few native vegetation remnant patches or scattered trees remaining.~~ Moreland's natural landscape is under pressure from a growing population, urban densification and climate change. Nevertheless, Moreland contains environmentally significant areas, in particular, local creeks and their surrounding environs.

Moreland's waterways are the distinctive natural feature of the municipality. waterways, which include the Moonee Ponds, Merri, Edgars, Westbreen and Merlynston Creek corridors, are not only an important natural resource but also provide an important opportunity for community recreation.

Urban consolidation, loss of backyards (which often provide good habitat linkages for fauna) and incremental loss of vegetation to infrastructure make it difficult to maintain biodiversity and reduce the urban heat island effect. As Moreland's population continues to grow and the climate warms, it will become increasingly important to mitigate heat, provide habitat refuge and biodiversity connectivity. ~~of habitat for biodiversity.~~

A healthy urban forest is central to the health and wellbeing of Moreland's community. Key ingredients for a healthy urban forest include soil health, water availability, free of pest and diseases, clean air, a healthy form and canopy, and an even age distribution of the forest.

Council seeks to improve and protect its environmental and landscape values by:

- Creating and protecting a diverse, connected and resilient environment of trees and other vegetation ~~urban forest of trees and other vegetation~~ that will enhance urban ecology and greening in both the public and private realm.
- Protecting and enhancing habitat corridors in parks and along waterways.
- Protecting the ecological integrity of the Merri, Moonee Ponds, Edgars, Westbreen and Merlynston Creek corridors and remaining areas of remnant vegetation areas.
- Encouraging development to be sensitive to all open space, river and creek interfaces.

02.03-7 Transport

Moreland is to be a connected City through a transport system that is diverse, progressive and sustainable that achieves a shift towards sustainable modes of travel, including zero emissions transport modes.

Car parking plays an active role in supporting broader transport and land use strategies. ~~To retain Moreland's liveability and manage transport issues associated with a denser built form, car parking will need to respond and adapt to a growing and changing Moreland.~~ The availability of car parking where people live, and their destinations, will strongly influence the ways they travel. Getting the type, location and amount of car parking right can contribute to better transport, land use, economic and community outcomes. This includes improved sustainable transport uptake while catering for those who need to drive.

Council seeks to create a transport system that is diverse, progressive and sustainable by:

- ~~Integrating transport and land use planning decisions that maximise people's opportunities to walk, cycle and use public transport.~~
- ~~Planning for car, bike and vehicle access and parking that:~~
 - ~~Creates better places within Moreland.~~
 - ~~Supports the likely demand and nature of the locality.~~
 - ~~Encourages the use of sustainable transport.~~
- Planning for a transport network that:
 - Caters for all ages, is accessible and equitable, including recognition that some people need to drive.
 - Reduces local vehicle traffic and safeguards the wellbeing of the community.
 - Achieves a shift towards sustainable modes of travel, including a transition to active transport or zero-emissions transport.
 - Focuses on transport safety, improving personal security and safety.
 - Connects people to local jobs and services.
 - Caters for population and employment growth.
- Prioritising our transport network according to the following 'road user hierarchy', while ensuring access for those who need to drive:
 - Pedestrians.
 - Cyclists.
 - Public transport.
 - Private vehicles.

Open Space Framework Plan

Hume City Council
















CITYLINK

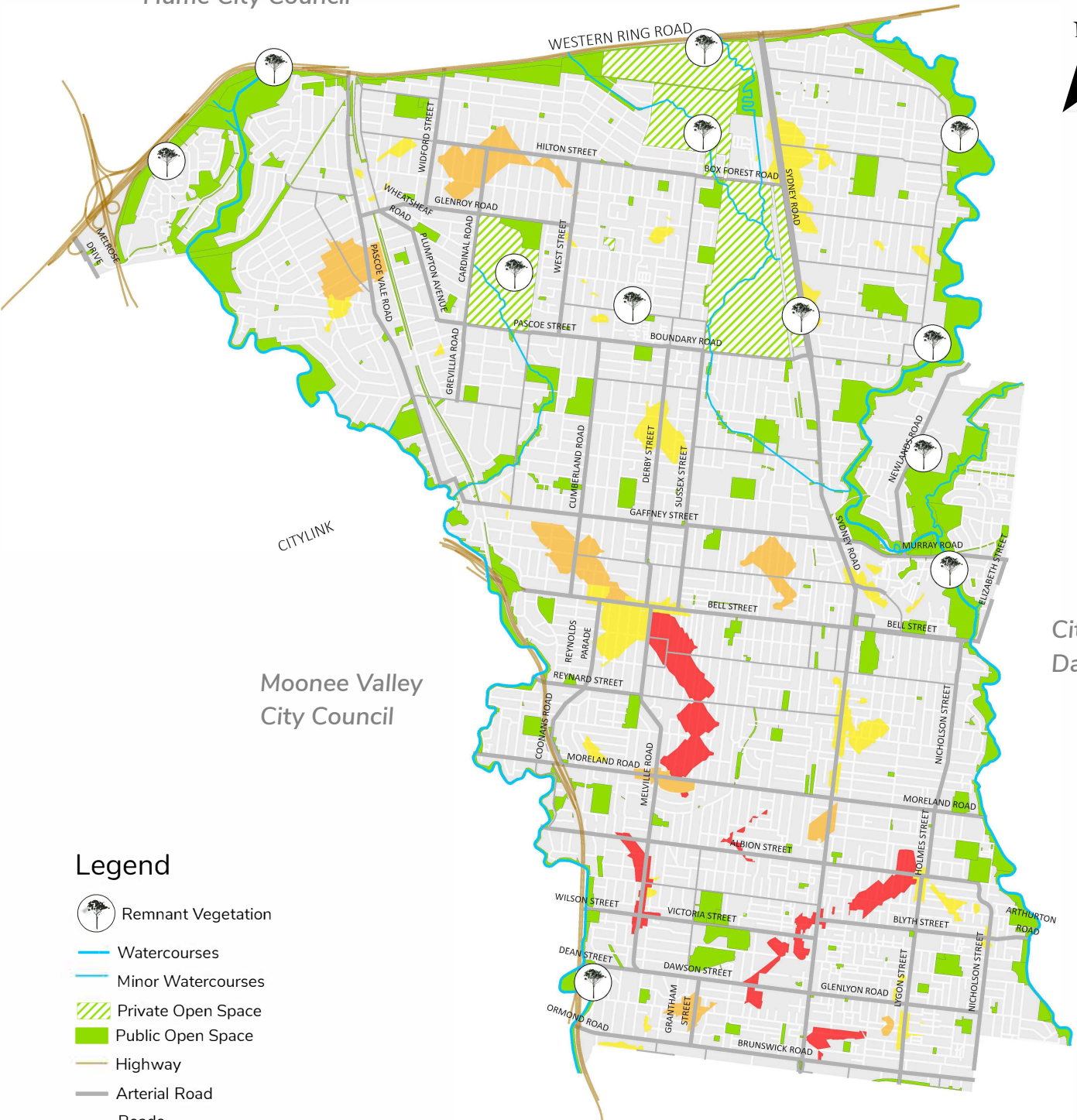
Moonee Valley City Council

City of Darebin

Legend

-  Remnant Vegetation
-  Watercourses
-  Minor Watercourses
-  Private Open Space
-  Public Open Space
-  Highway
-  Arterial Road
-  Roads
-  High Priority Open Space Gap Areas
-  Medium Priority Open Space Gap Areas
-  Low Priority Open Space Gap Areas
-  Streets
-  Moreland Local Government Area

0 1 2 3 4 Kilometers

11.03-1L Activity Centres in Moreland

--/--
C212

Strategies

Ensure residential uses do not undermine the viability of businesses operating in activity centres. ~~Residential amenity expectations should be consistent with activity centres' role to accommodate a mix of uses with day and night time activity.~~

Support the continued operation of existing noise generating uses, ~~including live music venues.~~

Policy Guideline

Consider as relevant:

- An activity centre's role to accommodate a mix of uses with day and night time activity. Consideration of residential amenity for dwellings within and adjacent to activity centres must be tempered to ensure the role of the activity centre is not undermined.

14/01/2021
C200

12.01-1L Biodiversity in Moreland

--/--
C212

Objective

To increase tree canopy cover and enhance the ecological values of the city.

Strategies

- [Encourage the planting and retention of trees in the private realm.](#)
- ~~Encourage~~ [Increase the planting and retention of native vegetation to enhance and create habitat corridors.](#)
- Design and site development to protect and enhance areas of remnant vegetation and indigenous revegetation as identified on Strategic Framework Plan: *Open Space*.
- [Encourage a diversity of vegetation to promote climate pest and disease resilience.](#)

15.01-1L

14/01/2024 --
/20--
C200more212

Urban design in Moreland

Strategies

Ensure site design, building frontages, design articulation and internal layout achieve a good interface with and surveillance of the public realm, including maximising opportunities for active frontages.

Design development to contribute to a fine grain urban structure reflecting an appropriate balance of open space to built-form.

Design development to contribute to a fine grain architectural expression with detailed street frontages.

Ensure large sites provide a network of public streets, footpaths and lanes connecting through the site into the surrounding street and pedestrian network.

Reallocate road space and existing car parking to create greener streets, such as through street tree planting.

Encourage development to contribute to the upgrade of existing streets adjoining the site and undergrounding of powerlines and other utilities.

Design development adjacent to public open space to provide a clear separation between public and private land.

Design and site development to not unreasonably overshadow public open space.

Design development to provide a sense of safety by maximising interaction, passive surveillance and incidental lighting of open spaces.

Ensure landscape design improves aesthetic quality and amenity for occupants and the public realm by:

- Integrating development with the surrounding environment.
- Providing for summer shading of buildings and private open spaces and allows for access to winter sun.
- Contributing to reduction of the urban heat island effect.
- Incorporating integrated water management and water sensitive urban design.

14/01/2021
C200

15.03-1L Heritage in Moreland

--/--/--
C212

Policy application

This policy applies to all land affected by a heritage overlay.

Demolition strategies

Encourage retention of contributory or significant heritage fabric required to maintain the original streetscape appearance.

Discourage total demolition of a contributory or significant heritage place unless:

- The building is structurally unsound, and that the contributory or significant heritage fabric has deteriorated beyond reasonable repair and would require reconstruction of the whole as shown through the submission of a structural engineering report prepared by a suitably qualified person.
- Any proposed replacement building makes a positive contribution to the heritage significance of the heritage place.

Ensure total demolition is not based on the grounds of poor condition or low integrity of a heritage place.

Support the demolition of non-contributory buildings in a heritage precinct, subject to the provision of a replacement building.

Support partial demolition of a heritage place, if:

- The fabric proposed to be removed does not contribute to the heritage significance of the place.
- The removal will enhance the significance of the place or facilitate conservation outcomes in accordance with the provisions of this policy.
- The extent of demolition will not result in facadism.

Discourage total reconstruction of a heritage place as an alternative to retention.

New buildings, alterations and additions strategies

Encourage new buildings and alterations and additions that:

- Respect the existing scale, massing, form and siting of contributory or significant elements and do not dominate the heritage place or precinct.
- Avoid alterations to the contributory or significant buildings (including new windows or door openings).
- Adopt innovative and contemporary design that makes a positive contribution to the heritage place.
- Do not imitate historic styles and detailing.

Ensure that new buildings, alterations and additions do not obscure important view lines to contributory or significant heritage buildings or their features.

Encourage the restoration or reconstruction of missing architectural elements that formed part of the original heritage fabric, if adequate evidence about its previous or original form is available.

Avoid using non-contributory and atypical built forms as a reference point for new works.

Encourage new buildings to be set back the same distance (or an average) from the front boundary as existing adjacent or nearby contributory or significant buildings.

Encourage new buildings to adopt side setbacks that reflect the side setbacks of adjoining or nearby contributory or significant buildings, where these setbacks are an important feature of the heritage place.

Encourage new buildings to adopt a facade height that is consistent with adjoining or nearby contributory or significant buildings.

Design alterations and additions (including upper storey additions) to contributory or significant heritage places to adopt the following:

- Additions to contributory heritage places should be visually recessive, including from the opposite side of any street (not a laneway) or public park abutting the property, and not dominate the heritage place.
- Additions to individually significant places should, as far as possible, be concealed by existing heritage fabric when viewed from the front and visually recessive when viewed from any other adjoining street (not a laneway).
- Existing facades, rooflines, chimneys or other elements that form part of the contributory or significant fabric of the heritage place should be preserved and not altered (such as new windows or door openings).

Ensure that, on corner sites, upper storey additions are distinct from, and read as secondary elements to the heritage place when viewed from the secondary street frontage.

Shopfronts and verandahs strategies

Encourage the conservation and enhancement of existing historic verandahs and shopfronts (including retrofitted types of later style and origin where this contributes to the significance of the heritage place).

Design alterations to existing contributory or non-contributory verandahs and shopfronts to be consistent with the character of the heritage place including:

- Accurate reconstruction of missing parts where evidence exists about the earlier state.
- A simplified, sympathetic form where no evidence exists about the earlier state.

Retain an existing shopfront if it is original or of later heritage value.

Vehicle accommodation and outbuildings strategies

Locate new car ports, garages and outbuildings in a location supported by the character of the heritage place and which does not dominate the heritage place such as either:

- To the rear of the main building.
- At the side of the building, set back from the main front building line (excluding verandahs, porches, bay windows or similar projecting features).

Discourage the construction of large hard surface areas within the front setback.

Discourage the creation of new or additional vehicle crossovers at the front boundary where this is not a feature of the heritage place or where rear laneway access exists.

Discourage the widening of existing vehicle crossovers except in the interest of traffic safety.

Encourage the reconstruction of kerb, channel and footpath where appropriate alternatives to existing front vehicle crossovers can be provided.

Front fences and gates strategies

Encourage retention and conservation of fences and gates that are a feature of the significance of the heritage place.

Encourage new fences that complement the style and period of construction of the heritage place in terms of height, materials and appearance.

Encourage visually permeable fences that do not obstruct view lines to a heritage place and its significant features when viewed from the street or other public spaces.

Encourage fences on side boundaries that are visible from the front street to adopt a similar scale to the front fence.

Ancillary services and equipment strategies

Ensure that ancillary services and equipment (such as satellite dishes, aerials, shade canopies, or similar structures) are concealed and do not detract from the heritage significance of the heritage place.

Encourage ancillary site services that support the sustainability of heritage fabric and reduce the operational environmental impact of the existing building (such as solar panels, water tanks, solar hot water systems, etc.) to be sensitively integrated into the design of the heritage place. These services may be visible, if there is no reasonable alternative location.

External materials, colours and finishes strategies

Encourage external materials, colours and finishes for heritage buildings that are consistent with and complement the style and period of the heritage place.

Discourage the decoration or concealment of existing unpainted surfaces.

Encourage the removal of paint from originally unpainted masonry surfaces by means that do not damage or alter the original surface.

Discourage sandblasting of render, masonry or timber surfaces.

Discourage rendering of previously unrendered surfaces.

Discourage the use of corporate colours on the body of a building unless such colours are consistent with the style of the building(s).

Streetscapes, landscapes and trees strategies

Discourage the removal of trees with an identified heritage value unless:

- The tree is dead or dying.
- The tree is causing structural damage to an existing heritage structure which cannot be dealt with by any other mean.
- Appropriate replacement landscaping is provided that respects the heritage place, maintains significant views and does not cause physical threat to existing heritage structures.

Ensure that new buildings or works do not adversely affect views and vistas toward, from and within heritage landscapes.

Ensure that new buildings or works do not present a physical threat to any tree with identified heritage value.

Ensure that new buildings or works are subservient to and maintain the prominence of the significant elements of the heritage landscape.

Retain significant elements of the heritage landscape.

Encourage the retention and conservation of heritage elements within streetscapes (including street trees, kerb and channel, laneways, etc.). New elements should be consistent with the historic character of the place.

Advertising signs strategies

Encourage advertising signs to adopt traditional locations on heritage buildings.

Discourage above-verandah, animated, bunting, electronic advertising, high wall, illuminated, major promotion, panel, pole, promotion, reflective and sky signs.

Ensure that the original architectural character of the building remains dominant.

Ensure that signs in heritage areas do not visually dominate, interrupt views to significant features or detract from the heritage significance.

Encourage traditional sign writing techniques on advertising signs.

Minimise the number of signs on a building.

Encourage signs that are proportional to the architectural features of the heritage place.

Encourage the retention of original signage.

Discourage signs that detract from, or obscure, important features of the heritage place.

Discourage the use of reflective, polished or metallic finishes on heritage places.

Discourage signs that project from the verandah or building.

Adaptive re-use of heritage places strategies

Allow a prohibited use only in those instances when that there are no viable alternatives.

Encourage the retention of as much contributory or significant fabric as possible where a change of use requires alterations to the heritage fabric.

Industrial heritage places strategies

Encourage the retention of contributory or significant heritage fabric unless retention undermines the continued viable operation of an industrial use.

Encourage the retention of as much contributory or significant heritage fabric as possible, if the site is redeveloped.

Subdivision strategies

Encourage subdivision that retains the existing pattern of development, if such a pattern contributes to the significance of the heritage place.

Ensure that significant landscape layouts, elements, trees and outbuildings are retained on title with the significant building.

Policy guidelines

Consider as relevant:

- Where demolition is to be supported, a visual record of any contributory or significant heritage fabric that is to be demolished or removed.
- The proposed relocation of a contributory or individually significant heritage building as total demolition.
- [The design approaches for alterations and additions contained in the *Guidelines for alterations and additions to detached houses in heritage areas*, Moreland City Council, ~~2001~~2021.](#)
- [The design approaches for fences contained in the *Guidelines for Fences in Heritage Areas*, Moreland City Council 2021.](#)
- The visibility of additions from the opposite side of any street (not a laneway) or public park abutting the property, taken from a height of 1.6 metres above ground level, unless the characteristics of the particular site make compliance unrealistic.
- Where adaptive re-use of a heritage place is proposed, conservation works to the heritage place concurrently or in advance of any new buildings and works being carried out.

Policy documents

Consider as relevant:

- [*Guidelines for alterations and additions to detached houses in heritage areas*, Moreland City Council, ~~2001~~2021.](#)
- [*Guidelines for Fences in Heritage Areas*, Moreland City Council, 2021](#)
- [*Guidelines for Assessing Planning Permit Applications*, Heritage Victoria, 2007](#)
- *Keeping Brunswick's Heritage: A Report on the Review of the Brunswick Conservation Study*. Context P/L, for Brunswick City Council, Vols. 1–4, 1990.

- *City of Coburg Heritage Conservation and Streetscape Study*. Timothy Hubbard P/L for City of Coburg, in five parts, October 1990–March 1991.
- *City of Moreland Heritage Review*. Allom Lovell and Associates for Moreland City Council, Vols. 1–5, January 1999.
- *Gallipoli Parade Heritage Precinct and Beaufort Houses - Review of Heritage Significance*, Context P/L for Moreland City Council, 2013.
- *Lygon Street Heritage Study Stage 2*, Context, June 2012.
- *Moreland Local Heritage Places Review*, Context P/L for Moreland City Council, 2004 (Amended April 2008).
- *North of Bell Street Heritage Study*, Context P/L for Moreland City Council, Vols. 1-2, 2013.
- *Moreland Heritage Gap Study*, Context P/L for Moreland City Council, Vols. 1-3, 2019.

14/01/2021
C200

16.01-4L Housing Affordability Moreland

--/--
C212

Strategies

Encourage developments to include affordable housing to be owned and managed by a registered housing association, registered housing provider or the Director of Housing.

[Encourage public landowners to use surplus land for the provision of social and affordable housing.](#)

14/01/2021
C200

17.01-1L Core Industry and Employment Areas

--/--
C212

Policy application

This policy applies to land identified as *Core Industry and Employment Areas* as identified on the Strategic Framework Plan: *Employment*.

Core Industry and Employment Areas Objective

To support the continued operation of existing industry and encourage new industry and complementary employment uses to locate within *Core Industry and Employment Areas*.

Core Industry and Employment Areas Strategies

Support the transition to a broader range of employment generating uses in *Core Industry and Employment Areas*, including a mix of industry and office-based uses and other compatible employment uses, as permitted within the relevant zone.

Discourage large scale retail (e.g. supermarkets greater than 1800 square metres) uses, other than restricted retail uses from locating in *Core Industry and Employment Areas*.

Encourage non-conforming uses, particularly residential uses, to convert to industrial or business uses.

Encourage the re-use of heritage protected buildings for industrial or business uses rather than residential uses.

Support the continued presence of the RMIT School of Fashion of Textiles within the *Brunswick Core Industry and Employment Area*.

Support 420 Victoria Street, Brunswick ([formally known as the Brunswick Business Incubator at 406 Victoria Street, Brunswick](#)) within the *Brunswick Core Industry and Employment Area* as a facility for entrepreneurs and enterprises.

Support the development of education and training facilities with a link to businesses within the *Core Industry and Employment Areas*.

Support showrooms, restricted retailing and wholesaling activities on main roads within *Core Industry and Employment Areas*, including:

- The Hume Highway Corridor north of Gaffney Street.
- Gaffney Street in the North *Coburg Core Industry and Employment Area*.
- Victoria, Albert and Dawson Streets in the *Brunswick Core Industry and Employment Area*.

Support the mixed-use nature of the precinct and surrounding areas while giving priority to industry and employment uses.

18.02-1L Sustainable Transport in Moreland

--/--
C200212

Strategies

Ensure the provision of bicycle parking is suitable to the likely demand generated by the use or development and nature of the locality.

Reallocating road space and existing car parking to:

- Support improvements to pedestrian and cycling infrastructure, including access to public transport.
- Facilitate the safety of walking and cycling.

18.02-2L Public Transport in Moreland

--/--/--
C212

Strategies

Reallocating road space and existing car parking to:

- Support improvements to public transport.

--/20--
C200

SCHEDULE TO CLAUSE 72.08 BACKGROUND DOCUMENTS

1.0

Background documents

--/20--
C212

Name of background document	Amendment number - clause reference
<i>A Park Close to Home</i> (Moreland City Council, December 2017)	
<i>Brunswick Structure Plan Reference Document</i> (Moreland City Council, 2018)	C167 2.03-1; 43.02s19
<i>Coburg Activity Centre Structure Plan Reference Document</i> (Moreland City Council, February 2018)	C167 2.03-1; 37.08s1
<i>Development Guidelines for Merri Creek</i> (Merri Creek Management Committee, 2004)	C152 12.03-1L
<i>Edgars Creek Conservation and Development Plan, Edwardes Lake to Merri Creek</i> (Thompson Berrill Landscape Design, June 2013)	C152 12.03-1L
<i>Gambling in Moreland 2015-2020</i> (Moreland City Council, 2015)	C163 52.28
<i>Glenroy Structure Plan</i> (Moreland City Council, 13 August 2008)	C109 2.03-1
<i>Guidelines for alterations and additions to detached houses in heritage areas</i> (Moreland City Council, 2004 2021)	C93 15.03-1L
<i>Guidelines for Assessing Planning Permit Applications</i> (Heritage Victoria, 2007)	C212 15.03-1L
<i>Guidelines for Fences in Heritage Areas</i> (Moreland City Council 2021)	C212 15.03-1L
<i>Livable Housing Design Guidelines</i> (Livable Housing Australia, 2017)	C152 16.01-3L
<i>Local impacts of electronic gaming machine (EGM) gambling in Moreland</i> (Moreland City Council, April 2011)	C157 52.28
<i>Moreland Activity Centre Framework</i> (Echelon Planning, February 2014)	C152 2.03-1
<i>Moreland Affordable Housing Strategy 2014-2018 Action Plan</i> (Moreland City Council, April 2014 September 2019)	C152 C212 2.03-5; 16.01-4L
<i>Moreland Apartment Design Code</i> (Moreland City Council, September 2015)	C142 15.01-2L
<i>Moreland Arts Infrastructure Plan 2018 – 2023</i> (Hodyl + Co. February 2018)	C212 2.03-6
<i>Moreland Bicycle Strategy 2011-2021</i> (Moreland City Council, November 2011)	C152 48.02-1L
<i>Moreland Climate Action Plan 2007</i> (Moreland City Council, April 2007)	C152 2.01
<i>Moreland Economic Development Strategy 2011-2016 2016-2021</i> (Moreland City Council, - August 2016)	C152 2.03-6
<i>Moreland Industrial Land Strategy 2015-2030</i> (Moreland City Council, July 2016)	C158

--/--/20--
C212




Name of background document	Amendment number - clause reference
	2.03-6; 16.01-1L; 17.01-1L
<i>Moreland Integrated Transport Strategy 2010-2019 (Moreland City Council, June 2010 <u>March 2019</u>)</i>	C152 <u>C212</u> 2.03-7; <u>18.02-1L</u> ; <u>18.02-2L</u>
<i>Moreland Integrated Water Management Plan 2010-2040 (Moreland City Council, 14 October 2009 <u>July 2020</u>)</i> <i><u>Moreland Nature Plan (Moreland City Council, 2020)</u></i>	C152 <u>C212</u> <u>C122</u> <u>2.03-2; 12.01-1L</u>
<i>Moreland Neighbourhood Centres Strategy 2017 (Moreland City Council, March 2017)</i>	C159 11.03-1L
<i>Moreland Open Space Strategy 2012-2022 (Moreland City Council, 2012)</i>	C122 2.01; 2.03-2; 2.03-8
<i>Moreland Parking Management Policy 2011 (Moreland City Council, February 2011)</i>	C152 18.02-4L
<i>Moreland Pedestrian Strategy 2010-19 (Moreland City Council, 8 September 2010)</i>	C152 2.03-7
<i>Moreland Tree Planting Manual for Residential Zones (Moreland City Council, 2014)</i>	C153 32.07s1; 32.07s2; 32.08s1; 32.08s2; 32.08s3; 32.08s4; 32.09s1
<i>Moreland Vehicle Crossing (Driveway) Policy 2010-2014 (Moreland City Council, July 2010 <u>April 2014</u>)</i>	C152 15.01-1L
<i><u>Moreland Zero Carbon 2040 Framework (Moreland City Council, September 2018)</u></i>	<u>C212</u> <u>2.01</u>
<i>Sydney Road and Upfield Corridor Strategic Framework Plan (David Lock Associates, 2014)</i>	C134 43.02s18
<i><u>Urban Forest Strategy 2017-2022 (Moreland City Council, 2017)</u></i>	<u>C212</u> <u>2.03-2; 12.01-1L</u>
<i>Urban Stormwater Best Practice Environmental Management Guidelines (CSIRO, 2006)</i>	C152 15.02-1L

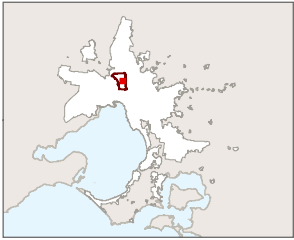
MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  IN1Z - Industrial 1 Zone
-  PPRZ - Public Park and Recreation Zone
-  Local Government Area



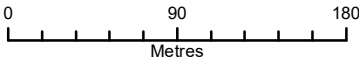
Part of Planning Scheme Maps 8 & 11

Disclaimer

This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group
Print Date: 6/01/2021
Amendment Version: 1





Environment,
Land, Water
and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  PPRZ - Public Park and Recreation Zone
-  Local Government Area

Disclaimer

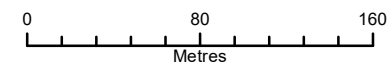
This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2020

Planning Group

Print Date: 23/11/2020

Amendment Version: 1



Environment,
Land, Water
and Planning

Part of Planning Scheme Map 5



MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

- PPRZ - Public Park and Recreation Zone
- Local Government Area



Part of Planning Scheme Maps 7 & 10

Disclaimer

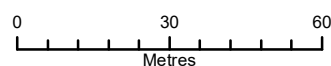
This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2020

Planning Group

Print Date: 27/11/2020

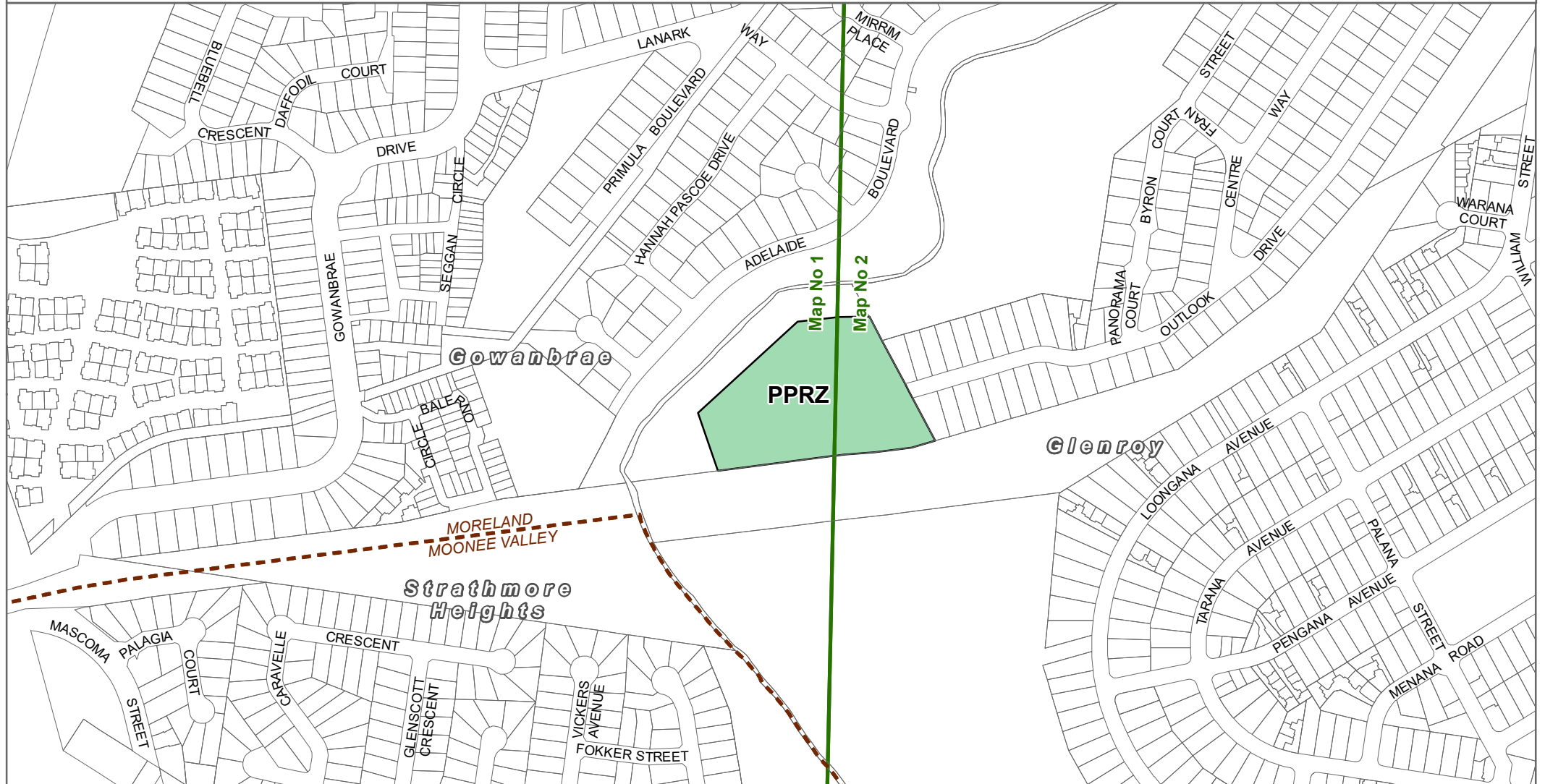
Amendment Version: 1



Environment,
Land, Water
and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

- PPRZ - Public Park and Recreation Zone
- Local Government Area

Disclaimer

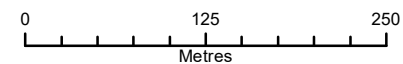
This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2020

Planning Group

Print Date: 23/11/2020

Amendment Version: 1



Environment,
Land, Water
and Planning

Part of Planning Scheme Maps 1 & 2



MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

- PPRZ - Public Park and Recreation Zone
- Local Government Area



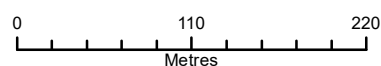
Part of Planning Scheme Map 9

Disclaimer

This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group
Print Date: 15/02/2021
Amendment Version: 1





Environment,
Land, Water
and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  PPRZ - Public Park and Recreation Zone
-  Local Government Area



Part of Planning Scheme Map 14

Disclaimer
 This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2020

Planning Group
 Print Date: 27/11/2020
 Amendment Version: 1

0 60 120
 Metres





Environment,
 Land, Water
 and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  EAO - Environmental Audit Overlay
-  Local Government Area



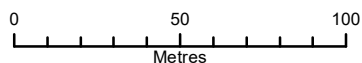
Part of Planning Scheme Map 14EAO

Disclaimer

This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group
Print Date: 24/03/2021
Amendment Version: 3





Environment,
Land, Water
and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION AMENDMENT C212more



LEGEND

-  PPRZ - Public Park and Recreation Zone
-  Local Government Area



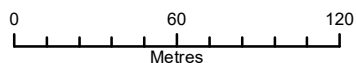
Part of Planning Scheme Map 14

Disclaimer

This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group
Print Date: 2/02/2021
Amendment Version: 1





Environment,
Land, Water
and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  EAO - Environmental Audit Overlay
-  Local Government Area

Disclaimer

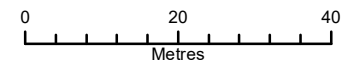
This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group

Print Date: 23/03/2021

Amendment Version: 3



Environment,
Land, Water
and Planning

Part of Planning Scheme Map 14EAO





MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  PPRZ - Public Park and Recreation Zone
-  Local Government Area

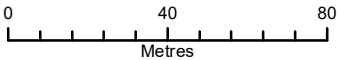


Part of Planning Scheme Map 3

Disclaimer
 This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2020

Planning Group
 Print Date: 23/11/2020
 Amendment Version: 1



Environment,
 Land, Water
 and Planning

MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

- C1Z - Commercial 1 Zone
- PPRZ - Public Park and Recreation Zone
- Local Government Area



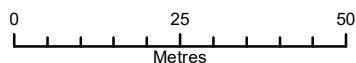
Part of Planning Scheme Map 15

Disclaimer

This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group
Print Date: 2/02/2021
Amendment Version: 1



Environment,
Land, Water
and Planning

17.01-1L

14/01/2024 --/--
/20--
C200more.
Proposed C212

Employment Areas**Policy application**

This policy applies to land identified as Employment Areas as identified on the Strategic Framework Plan: ~~Employment~~ Economic Development.

Employment Areas Objective

To support the transition from traditional industrial uses to a broader range of employment uses.

Employment Areas Strategies

Support the continued operation of existing viable industries in *Employment Areas*.

Facilitate the transition of *Employment Areas* to accommodate a broader range of employment generating uses, including a mix of industry and office-based uses and other compatible employment uses.

Prioritise employment uses over residential uses either by:

- Ensuring the amount of employment floor space in a mixed-use development is at least equivalent to the amount of all proposed ground and first floor building areas (inclusive of car parking, other services and circulation space).
- Allocating a proportion of the site to employment uses and including the area in a zone that prohibits residential use where rezoning of a large site is proposed.

Encourage flexible floor plate layouts and increased floor to ceiling heights at ground and first floor to facilitate a variety of employment uses over time.

Encourage building design and site layouts of industrial and commercial uses in *Employment Areas* to minimise the potential for adverse amenity and environmental impacts on residential uses.

Design and layout new residential and mixed-use development to incorporate design and noise attenuation measures to protect residents from noise from nearby businesses, such as locating bedrooms away from noise sources and using acoustic measures.

Support the mixed-use nature of the precinct and surrounding areas while giving priority to employment uses.

03/08/2017
C163

SCHEDULE 16 TO THE DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO16**.

GAFFNEY STREET PRECINCT - 14-22 GAFFNEY STREET, COBURG NORTH

1.0

13/02/2014
C34

Design Objectives

- To sensitively manage interface issues between new development and established areas.
- To encourage the creation of a high quality public realm with pedestrian friendly environment and a hierarchy of streets with active street frontages along Gaffney Street and the Upfield Bike Path.
- To promote buildings of high architectural quality and visual interest to the street.
- To ensure new development has flexible floor spaces that can sustain a variety of business operations and residential accommodation where appropriate.
- To ensure that development addresses and provides passive surveillance to Gaffney Street, the Upfield Bike Path, communal open spaces and primary internal streets.
- To ensure new development protects and enhances the Upfield Bike Path.
- To ensure traffic and public transport related impacts are considered and mitigated as appropriate.
- To ensure new development provides noise attenuation measures where required.
- To ensure that development incorporates best practice environmentally sustainable design initiatives.
- To establish convenient, safe and efficient movements and connections within the development and to the existing street network and transport opportunities.
- To ensure that development incorporates best practice accessible design features to meet the access needs of all sectors of the community.
- To provide high quality internal amenity for occupants including access to good natural light to all habitable rooms.

2.0

13/02/2014
C34

Application requirements

An application for development or subdivision of the subject site must include, as appropriate, the following information to the satisfaction of the Responsible Authority:

- An urban context report including site context plan and design response.
- A written statement that demonstrates how the proposed development achieves the design objectives and buildings and works requirements of this schedule.
- An Environmentally Sustainable Design Management Plan (EMP) prepared by a suitably qualified environmental engineer or equivalent that demonstrates how the development provides for best practice environmentally sustainable design.
- An Acoustic Engineering Report prepared by a suitably qualified person demonstrating the use of suitable materials and building treatments to ensure internal noise levels are satisfactory.
- A Circulation and Movement Plan prepared by a suitably qualified person demonstrating the internal road hierarchy including primary and secondary roads and areas to be accessed by loading and building services, vehicles, pedestrians and cyclists.
- An Accessibility Report prepared by a suitably qualified person detailing how the development will incorporate adaptable, accessible and visitable design features, in accordance with *Australian Standard 4299-1995: Adaptable Housing*.

3.0

13/02/2014
C34

Permit Requirements

A permit is required to construct a fence.

4.0

11/02/2021
C199more

Buildings and Works

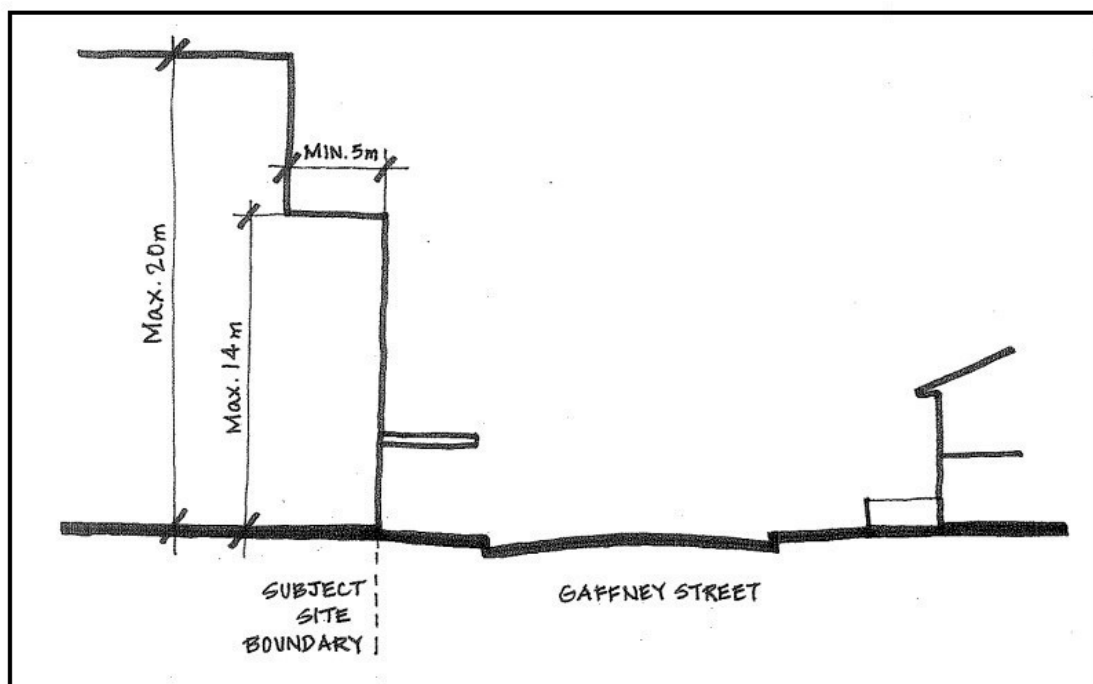
Heights and Setbacks

Where maximum preferred building heights are not specified, building heights should not exceed 25.2 metres in height.

Gaffney Street interface

- A maximum height of 20 metres only is permitted for buildings fronting Gaffney Street that incorporates, in accordance with Figure 1 below:
 - A zero lot line podium with a maximum height of 14 metres, that supports active frontages to Gaffney Street; and
 - Built form above podium height that is set back a minimum of 5 metres.

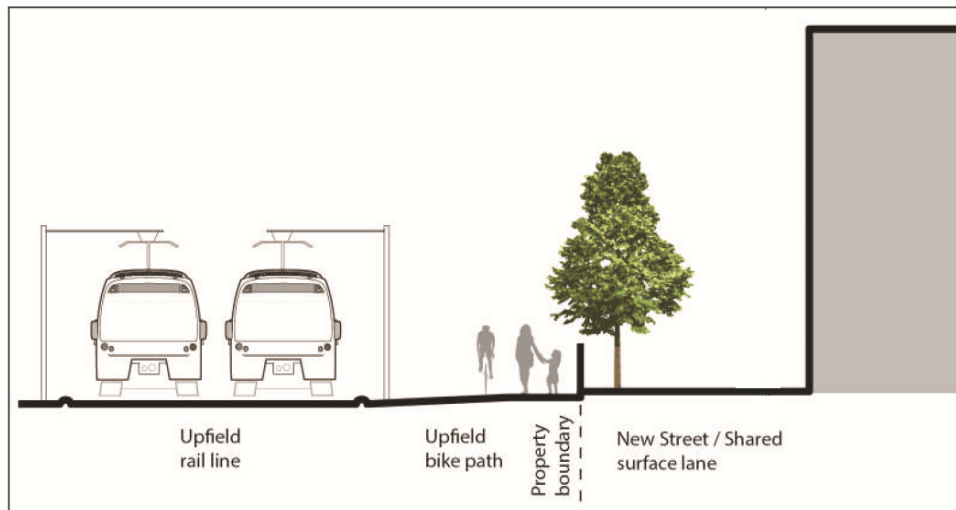
Figure 1. Gaffney Street interface



Upfield rail corridor interface

- Buildings that have an interface with the Upfield railway corridor should respond to the rail line in accordance with figure 2 below.

Figure 2. Upfield rail corridor interface



- Buildings (excluding the boundary fence, road and landscaping furniture) that have an interface with the Upfield railway corridor must be set back no less than 10 metres from the centre line of the existing eastern railway track.
- Visual and functional connection of new development with Batman Station and the Upfield Bike Path should be provided to facilitate activation of this interface.
- Built form that creates an interesting skyline at the interface with the Upfield rail corridor.

North and east site interfaces with General Residential Zone land

- Buildings that have interfaces with residential properties at the north and east site boundaries should provide building setbacks in accordance with Figures 10 and 11 of the *Urban Design Framework, 14-22 Gaffney Street Coburg North*.

Design and Articulation

- Development should be appropriately articulated and modulated and provide visual interest through glazing and variation in materials and textures.
- Development along all existing and new primary road frontages and communal open spaces should be designed to ensure activation with all levels providing a high degree of passive surveillance of adjacent public areas.
- Avoid high fencing, where provided high fencing must be visually permeable.
- Buildings fronting Gaffney Street must provide:
 - Built scale appropriate to the street and proximity of pedestrians.
 - Windows facing Gaffney Street of at least 80% of the width of any commercial premises.
 - Maximise the number of entries to any ground floor residential dwellings.
 - Clear glazing to street frontages.
 - Facade design that incorporates lighting to add to a sense of security at night.
 - The provision of roller doors is not encouraged.
 - Visually permeable or transparent security grilles, which are encouraged to be internally mounted.
 - A suitable interface with the adjacent existing bus stop on Gaffney Street having regard to any proposed road upgrades, to the satisfaction of the Public Transport Development Authority and Roads Corporation.

MORELAND PLANNING SCHEME

- Loading facilities and building services should not be located on existing or new primary road frontages, unless they can be visually integrated into the site.
- Vehicle ingress and egress to individual dwellings should not be located on existing or new primary road frontages.
- Development should minimise blank walls, large service areas, continuous garage doors and dense planting at ground level street frontages.
- Development incorporating ten or more dwellings must be designed to incorporate adaptable, accessible and visitable design features.
- Development is encouraged to incorporate public art that celebrates the cultural heritage significance of the area.
- Main entries and lobby spaces should be legible from the street frontage with good physical and visual connections to the street.
- Development should be designed to provide passive surveillance of primary and secondary streets and courtyards including through the orientation of balconies and openings of habitable rooms.

Integrated development

- Where development is staged, each stage of the development should be integrated with the previous stage and where appropriate provide opportunity for integration with future adjacent stages.

Landscaping and public realm

- Development should ensure appropriate street tree planting, landscape concepts and street furniture are provided along all accessways and pedestrian links.
- Canopies along Gaffney Street must not preclude street tree planting.

Noise attenuation

- Building and works must be sited and designed to incorporate noise attenuation measures to minimise railway noise levels or other external noise sources within a development: This may be achieved by:
 - Providing adequate soundproofing where noise levels are expected to exceed ambient levels.
 - Using noise resistant materials and construction methods.
- Noise attenuation measures should be taken into account when designing external courtyard, balcony and recreation areas and other sensitive amenity areas.
- Noise attenuation measures must be integrated with appropriate urban design outcomes to achieve articulated diverse facades and active frontages.

Movement and Access

- Any development must include the creation of an accessway on the western boundary of the property to provide an interface with the railway line, provide street planting opportunities and incorporate or provide upgrades to the existing Upfield Bike Path.
- Any development must seek to protect and enhance the Upfield Bike Path to improve pedestrian amenity, bicycle access and the active movement network.
- Development should provide for the connection of new roads and laneways to the existing roads and laneways, to create a highly permeable and accessible environment.
- Primary vehicular access to the site must be provided from Gaffney Street.

- Development must provide for a pedestrian link between the south-west corner of the site adjacent to the Upfield Bike Path / Batman Train Station and the laneway in the north-east corner of the site connecting to Sydney Road.
- Roads and laneways must make appropriate provision for vehicular traffic, safe pedestrian access for all, bicycles and street tree landscaping.
- Where practicable, redevelopment of the site should provide shared zones for pedestrians, cyclists and vehicles.

Environmentally sustainable design

- Any development on the site must incorporate environmental sustainable design measures in the areas of energy efficiency, greenhouse gas emissions, integrated water management, natural ventilation, storm water reduction and management, solar access, orientation, transport, waste reduction/management and construction materials selection.
- Development must provide an overall road layout that maximizes the opportunity for solar efficient lot layouts and building orientation within the site and that does not compromise the existing passive solar access of existing dwellings surrounding the site.

5.0

13/02/2014
C34

Subdivision

Any application to subdivide land must include a Plan of Subdivision showing roads, pedestrian links and any public open space.

An application to subdivide land that will impact on achieving the objectives of the Design and Development Overlay or would prejudice the implementation of the Integrated Transport Plan, as required in the Section 173 Agreement registered on title, will not be supported.

6.0

13/02/2014
C34

Decision guidelines

Before deciding on an application, the Responsible Authority must consider as appropriate:

- The design objectives of this schedule.
- The Urban Design Framework, 14-22 Gaffney Street Coburg North.
- The architectural quality and innovative response of the buildings.
- The contribution the design makes to the urban design, walkability, bicycle access, permeability and streetscape appearance of the area.
- Whether the development incorporates appropriate noise attenuation measures to minimise railway noise levels or other relevant external noise sources within the development having regard to the proposed use of the land.
- The overall environmental sustainability performance of development.
- Whether the proposed development implements the outcomes outlined in the Integrated Transport Plan.
- Whether the proposed development provides the works that are required to the public transport and road networks in the vicinity of the site to mitigate against the impacts arising from the development of the site.
- The views of the Public Transport Development Authority and Roads Corporation.

7.0

Reference Documents ~~(4)~~

11/02/2021 - / - / - - -
G499more Proposed C212more Urban Design Framework, 14-22 Gaffney Street Coburg North, David Lock Associates, June 2013.



MORELAND PLANNING SCHEME

*Moreland Industrial Land ~~Use Strategy~~ Strategy 2015-2030, Hansen Partnership, National
Economics, August 2004* Moreland City Council, July 2016.

*14-22 Gaffney Street, Coburg: Rezoning Application, Traffic and Car Parking Impact Assessment
Report, Traffix Group, January 2012.*





MORELAND PLANNING SCHEME - LOCAL PROVISION

AMENDMENT C212more



LEGEND

-  HO - Heritage Overlay
-  Local Government Area



Part of Planning Scheme Map 14HO

Disclaimer
 This publication may be of assistance to you but the State of Victoria and its employees do not guarantee that the publication is without flaw of any kind or is wholly appropriate for your particular purposes and therefore disclaims all liability for any error, loss or other consequence which may arise from you relying on any information in this publication.

© The State of Victoria Department of Environment, Land, Water and Planning 2021

Planning Group
 Print Date: 12/01/2021
 Amendment Version: 1

0 50 100
 Metres



Environment,
 Land, Water
 and Planning

2.0

Heritage places⁽⁺⁾

48/42/2020-/-/-----
G211moreProposed C212more

The requirements of this overlay apply to both the heritage place and its associated land.



PS Map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO431	Precinct Adler Grove Precinct, Coburg North Map 8HO Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019	No	No	No	No	No	No	No
HO1	Precinct Albert Street Precinct, Brunswick Map 15HO Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019	No	No	No	No	No	No	No
HO384	Precinct Allan Street Precinct, Brunswick Map 15HO Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019	No	No	No	No	No	No	No
HO432	Precinct Bain Avenue Precinct, Coburg North	No	No	No	No	No	No	No

MORELAND PLANNING SCHEME

PS Map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019							
HO155	1-3 Dawson Street, Brunswick – St Ambrose's School & Hall Map 14HO Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019	Yes	No	No	No	No	Yes	No
HO61	10 Dawson Street, Brunswick – Brunswick Baths Map 14HO Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019	Yes	No	No	No	No	Yes	No
HO246	Latiner Hat Factory (former) , 20 Dawson Street, Brunswick Map 14HO Incorporated Plan: Moreland Heritage Exemptions Incorporated Plan 2019 Statement of Significance: <i>'Latiner Hat Factory, 20 Dawson Street, Brunswick, Statement of Significance, September 2019'</i>	No	No	No	No	No	No	No
HO62	49 Dawson Street, Brunswick – Former Brunswick Technical School	Yes	No	No	No	No	Yes	No

SCHEDULE TO CLAUSE 72.04 DOCUMENTS INCORPORATED IN THIS PLANNING SCHEME

1.0

Incorporated documents⁽¹⁾

48/42/2020 - / - / - - -
G211more Proposed C212more

Name of document	Introduced by:
13-17 Duggan Street, Brunswick West Incorporated Plan – June 2003	C25
21-35 Hope Street, Brunswick Incorporated Plan – September 2003	C36
Bell and Moreland Level Crossing Removal Project Incorporated Document, October 2019	C178more
Brunswick Terminal Station Incorporated Document, 2012	C140
Camp Road, Campbellfield Level Crossing Removal Project Incorporated Document, April 2017	GC63
City of Moreland Residential Development at 42 St Phillip Street, Brunswick East 23 May 2003	C44
Geotechnical Declaration and Certification Form, Moreland City Council, November 2012	C83
King Khalid Islamic College Primary School Campus Incorporated Plan - May 2004	C41
Melbourne City Link Project – Advertising Sign Locations, November 2003	VC20
Moreland Heritage Exemptions Incorporated Plan 2019	C174morept1
Moreland Tree Planting Manual for Residential Zones, December 2014	C153
Pentridge Coburg Design Guidelines and Masterplan, February 2014	C154
Pentridge Village Design Guidelines and Masterplan, August 2009	C125
Moreland Development Contribution Plan, January 2015	C133
Signage on sports grounds Incorporated Document, June 2019	C169more
Statements of Significance Incorporated under Clause 43.01	
<ul style="list-style-type: none"> Attached Houses, 23 & 25 Luscombe Street, Brunswick, Statement of Significance, September 2019 	C174morePt1
<ul style="list-style-type: none"> Attached Houses, 46 & 48 Gold Street, Brunswick, Statement of Significance, September 2019 	C174morePt1
<ul style="list-style-type: none"> Attached Houses, 60 & 62 Glenlyon Road, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> Bell Street Bridge, Corner of Bell Street and Nicholson Street, Coburg, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> Bethany Gospel Hall (former), 235 Victoria Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> Brunswick & Coburg UFS Dispensary (former), 228 Melville Road, Brunswick West, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> 'Canals House', 7 Parkview Avenue, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> 'Closer Settlement House ('Ranche') 9 Waxman Parade, Brunswick West, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> Closer Settlement Houses, Statement of Significance, September 2019 	C174morept1

MORELAND PLANNING SCHEME

Name of document	Introduced by:
<ul style="list-style-type: none"> ▪ Coburg Police Station (former), 160-162 Bell Street, Coburg, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Concrete House, 1 South Daly Street, Brunswick West, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Corner Shop (former) & Residence, 80 Union Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Corner Shop (former) & Residence, 103 Albion Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Corner Shop (former) & Residence, 209 Barkly Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Dairy & Milk Bar (former), 136A Nicholson Street, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ 'Devon' (House), 38 John Street, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Faiman & Sons Warehouse (former), 443 Albion Street, Brunswick West, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ First City of Brunswick Scout Hall (former), later Brunswick Cinema, Toscana Social Club, 273 Victoria Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ 'Fleming Park,' 47-51 Albert Street, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ 'Forfarshire' (House), 86 Laura Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Former Millers Ropeworks, now RMIT University, 29 Dawson Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Former Prestige Mill Complex, 159-165 Donald Street, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Former W.T Rawleigh and Co – factory & warehouse, 60 Dawson Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ 'Glen Iris' & 'Hazel Glen', 28 & 30 Albert Street, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ Hickford Street Precinct, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ House, 5 Bank Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ House, 5 Laura Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ House, 8 Aintree Street, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ House, 19 Balmoral Avenue, Brunswick East, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ House, 23 Evans Street, Brunswick, Statement of Significance, September 2019 	C174morept1
<ul style="list-style-type: none"> ▪ House, 23 Holmes Road, Brunswick East, Statement of Significance, September 2019 	C174morept1

MORELAND PLANNING SCHEME

Name of document	Introduced by:
▪ House, 31 Burnell Street, Brunswick West, Statement of Significance, September 2019	C174morept1
▪ House, 38 David Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 46 Murdock Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 94 Hope Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 96 Lydia Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 112 Moreland Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 137 Albert Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 195 Barkly Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 307 Brunswick Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House, 685 Park Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ House & Canary Island Palm, 255 Nicholson Street, Brunswick East, Statement of Significance, September 2019	C174morept1
▪ House & Front Fence, 33 Newman Street, Brunswick West, Statement of Significance, September 2019	C174morept1
▪ House & Front Fence, 22 Grantham Street, Brunswick West, Statement of Significance, September 2019	C174morept1
▪ House & Front Fences, 92 & 94 Whitby Street, Brunswick West, Statement of Significance, September 2019	C174morept1
▪ Houses, 63 & 65 Union Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ H.P.L. Morris Chemist (former) & Residence, 153 Reynard Street, Coburg, Statement of Significance, September 2019	C174morept1
▪ 'Kororoit' (Concrete House), 70 Heller Street, Brunswick West, Statement of Significance, September 2019	C174morept1
▪ 'Lochiel', 9 Aberdeen Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ 'Lough Brae' (House) 35 Cassels Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Meaker Avenue Precinct, Statement of Significance, September 2019	C174morept1
▪ 'Moorefield' (Outbuilding), 2/197 The Avenue, Coburg, Statement of Significance, September 2019	C211more
▪ 'Moreland Baptist Church Complex' 384 Moreland Road, Brunswick West, Statement of Significance, September 2019	C174morept1

MORELAND PLANNING SCHEME

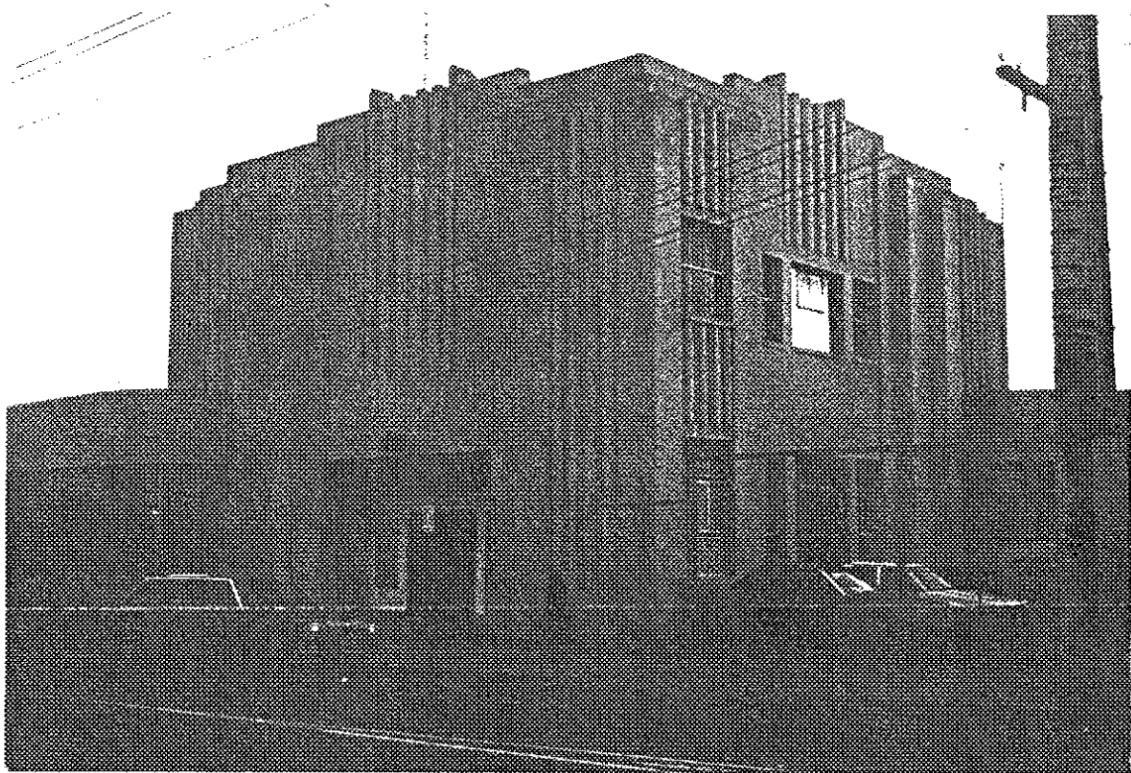
Name of document	Introduced by:
▪ 'Oamaru' (Aitken House), 26 Walsh Street, Coburg, Statement of Significance, September 2019	C174morept1
▪ 'Oomah' (House), 73 Hope Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Primitive Methodist Mission Hall, 170 Albert Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Rogers Memorial Reserve, 109 Cumberland Road, Pascoe Vale, Statement of Significance, September 2019	C174morept1
▪ Shop (former) & Residence, 105 Munro Street, Coburg, Statement of Significance, September 2019	C174morept1
▪ Shop & Residence, 425 Sydney Road, Coburg, Statement of Significance, September 2019	C174morept1
▪ Shops & Residences, 431 & 433 Sydney Road, Coburg, Statement of Significance, September 2019	C174morept1
▪ State Savings Bank of Victoria (former), 840 Sydney Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace, 163-169 Barkly Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace, 183-187 Edward Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace, 206-210 Barkly Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terraces, 233-239 Brunswick Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses, 16 & 18 Glenlyon Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses (23 – Canaway House, 25 – Albert House) 23 & 25 Albert Street, Brunswick East, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses (31 – Inverurie, 33 – Keithall), 31 & 33 Lygon Street, Brunswick East, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses, 109 & 111 Albert Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses, 140 & 142 Edward Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses (213 - Ethel, 215 – Edith, 217 – Alice, 219 – Betty), 211-219 Barkly Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses, 241 & 243 Victoria Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses & Fences, 76 & 78 Hope Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Terrace Houses & Fences, 134-136 Barkly Street, Brunswick, Statement of Significance, September 2019	C174morept1

MORELAND PLANNING SCHEME

Name of document	Introduced by:
▪ 'The Elms' (House), 683 Park Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ Tinning Street Precinct, Statement of Significance, September 2019	C174morept1
▪ 'Tyrone' (House), 193 Brunswick Road, Brunswick, Statement of Significance, September 2019	C174morept1
▪ 'Uxbridge' (House) 16 Horne Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ 'Victoria Hotel', 380-382 Victoria Street, Brunswick, Statement of Significance, September 2019	C174morept1
▪ War Service Homes (Brick), Statement of Significance, September 2019	C174morept1
▪ War Service Homes (Timber), Statement of Significance, September 2019	C174morept1
▪ Waxman Parade Precinct, Statement of Significance, September 2019	C174morept1
▪ Yorkshire Textile Mills (Former), 2-4 Inverness Street, Brunswick East, Statement of Significance, September 2019	C174morept1
▪ Latiner Hat Factory, 20 Dawson Street, Brunswick, Statement of Significance, September 2019	C212more
Tramway Infrastructure Upgrades Incorporated Document, May 2017	GC68

Statement of Significance: Latiner Hat Factory, 20 Dawson Street, Brunswick

Heritage Place:	'Latiner Hat Factory (former),' 20 Dawson Street, Brunswick	PS ref no:	HO246
------------------------	---	-------------------	-------



What is significant?

The former Latiner Hat Factory, designed by Alder & Lacey and constructed in 1933-34, at 20 Dawson Street, Brunswick is significant. This is a large interwar factory in the Moderne style. The long Dawson street elevation comprises the three-storey office 'tower' at the eastern end, and the long single storey factory section extending to the west. The office 'tower' has a stepped, cubic form with vertical fins reminiscent of a Sumerian temple. The only opening in monolithic façade to Dawson Street is the wide doorway under the central bay, while the eastern and western elevations contain several multi-pane metal-framed windows. On the east side, and set back from the street, is a single storey section with a continuous band of multi-paned metal-framed windows and incised horizontal bands to the parapet.

The key feature of the façade of the factory block extending to the west is an almost continuous band of metal framed windows, uninterrupted by piers and broken only by door and vehicle openings. The saw tooth roofs behind are hidden by the parapet. This building terminates at an internal roadway. On the opposite site is another building, again with multi-pane metal framed windows across the façade and saw tooth roofs behind, with fully glazed side walls facing the internal road.

How is it significant?

The former Latiner Hat Factory at 20 Dawson Street, Brunswick is of local historical and aesthetic significance to the City of Moreland.

Why is it significant?

It is significant as one of the large factories established in response to the introduction of tariff protection in the early twentieth century and illustrates the boom in manufacturing that made Brunswick a centre of industry in Melbourne. It is representative of the large modern architect-designed factory complexes that emerged during the 1930s and is notable as one of the most intact and complete surviving examples in Brunswick. (Criteria A & D)

It is significant as a fine example of the Moderne style applied to an industrial building. The Moderne style was popular for factories in the 1930s as it was seen as progressive and modern. This factory has all the hallmarks of the style including the strong horizontal emphasis of the factory section punctuated by the strong vertical tower at one end. The construction technique, which enabled the façade windows to be expressed as an almost continuous horizontal band uninterrupted by piers and the striking form and detailing of the office with its stepped cubic monolithic façade relieved by vertical fins are of particular note. (Criterion E)

Primary source

Moreland Heritage Gap Study 2019, Vol 1: Findings and Methodology Report

Northern Factory Study Part 2, 1992